

LOCATION MAP
 NOT TO SCALE

| INDEX OF DRAWINGS | | |
|-------------------|---|----------------|
| SHEET | DRAWING NAME | DRAWING NUMBER |
| 1 | TITLE SHEET | TITLE |
| 2 | GENERAL NOTES | NTS-1 |
| 3 | QUANTITIES | QTY-1 |
| 4 | TABLES (1 OF 2) | TAB-1 |
| 5 | TABLES (2 OF 2) | TAB-2 |
| 6 | BOWEN ROAD PLANS (1 OF 11) | BOP-1 |
| 7 | BOWEN ROAD PLANS (2 OF 11) | BOP-2 |
| 8 | BOWEN ROAD PLANS (3 OF 11) | BOP-3 |
| 9 | BOWEN ROAD PLANS (4 OF 11) | BOP-4 |
| 10 | BOWEN ROAD PLANS (5 OF 11) | BOP-5 |
| 11 | BOWEN ROAD PLANS (6 OF 11) | BOP-6 |
| 12 | BOWEN ROAD PLANS (7 OF 11) | BOP-7 |
| 13 | BOWEN ROAD PLANS (8 OF 11) | BOP-8 |
| 14 | BOWEN ROAD PLANS (9 OF 11) | BOP-9 |
| 15 | BOWEN ROAD PLANS (10 OF 11) | BOP-10 |
| 16 | BOWEN ROAD PLANS (11 OF 11) | BOP-11 |
| 17 | NORTH FOREST ROAD TYPICAL SECTIONS (1 OF 3) | NFS-1 |
| 18 | NORTH FOREST ROAD TYPICAL SECTIONS (2 OF 3) | NFS-2 |
| 19 | NORTH FOREST ROAD TYPICAL SECTIONS (3 OF 3) | NFS-3 |
| 20 | NORTH FOREST ROAD PLANS (1 OF 12) | NFP-1 |
| 21 | NORTH FOREST ROAD PLANS (2 OF 12) | NFP-2 |
| 22 | NORTH FOREST ROAD PLANS (3 OF 12) | NFP-3 |
| 23 | NORTH FOREST ROAD PLANS (4 OF 12) | NFP-4 |
| 24 | NORTH FOREST ROAD PLANS (5 OF 12) | NFP-5 |
| 25 | NORTH FOREST ROAD PLANS (6 OF 12) | NFP-6 |
| 26 | NORTH FOREST ROAD PLANS (7 OF 12) | NFP-7 |
| 27 | NORTH FOREST ROAD PLANS (8 OF 12) | NFP-8 |
| 28 | NORTH FOREST ROAD PLANS (9 OF 12) | NFP-9 |
| 29 | NORTH FOREST ROAD PLANS (10 OF 12) | NFP-10 |
| 30 | NORTH FOREST ROAD PLANS (11 OF 12) | NFP-11 |
| 31 | NORTH FOREST ROAD PLANS (12 OF 12) | NFP-12 |
| 32 | WORK ZONE TRAFFIC CONTROL DETAILS (1 OF 9) | WZD-1 |
| 33 | WORK ZONE TRAFFIC CONTROL DETAILS (2 OF 9) | WZD-2 |
| 34 | WORK ZONE TRAFFIC CONTROL DETAILS (3 OF 9) | WZD-3 |
| 35 | WORK ZONE TRAFFIC CONTROL DETAILS (4 OF 9) | WZD-4 |
| 36 | WORK ZONE TRAFFIC CONTROL DETAILS (5 OF 9) | WZD-5 |
| 37 | WORK ZONE TRAFFIC CONTROL DETAILS (6 OF 9) | WZD-6 |
| 38 | WORK ZONE TRAFFIC CONTROL DETAILS (7 OF 9) | WZD-7 |
| 39 | WORK ZONE TRAFFIC CONTROL DETAILS (8 OF 9) | WZD-8 |
| 40 | WORK ZONE TRAFFIC CONTROL DETAILS (9 OF 9) | WZD-9 |
| 41 | MISCELLANEOUS DETAILS | MDS-1 |



**ERIE COUNTY
 DEPARTMENT OF PUBLIC WORKS
 DIVISION OF HIGHWAYS**

**PLANS FOR THE 2026 OVERLAY B
 BOWEN ROAD - CR 242, 3.27 MI.
 NORTH FOREST ROAD - CR 294, 3.29 MI.
 EC PROJECT NUMBER
 CAP-242.294-26**

**TOWN OF ELMA AND
 TOWN OF AMHERST
 COUNTY OF ERIE
 STATE OF NEW YORK**

TYPE OF CONSTRUCTION
 THE PROJECT INVOLVES A 3½" MILL AND OVERLAY ON NORTH FOREST ROAD IN THE TOWN OF AMHERST AND 1½" TOP COURSE AND STRIPING ON BOWEN ROAD IN THE TOWN OF ELMA.

REFER TO NYSDOT STANDARD SHEETS:
 402-01
 619-002, 619-004, 619-010, 619-011, 619-012, 619-307, 619-310, 619-311, 619-312,
 619-313, 619-321, 619-323, AND 619-412
 680-14
 685-01

ALL WORK PERFORMED UNDER THIS CONTRACT IS TO BE IN CONFORMANCE WITH THE "STANDARD SPECIFICATIONS (US CUSTOMARY UNITS)" OF THE NEW YORK STATE DEPARTMENT OF TRANSPORTATION, DATED MAY 1, 2026, INCLUDING CURRENT ADDITIONS AND MODIFICATIONS, AS WELL AS THOSE MADE BY THE ERIE COUNTY DEPARTMENT OF PUBLIC WORKS, DIVISION OF HIGHWAYS, EXCEPT AS MODIFIED BY THESE PLANS, THE ITEMIZED PROPOSAL, AND THE INSTRUCTIONS TO BIDDER.

NOTICE PROJECT MANUALS, DRAWINGS AND CONSTRUCTION DOCUMENTS ARE INTENDED SPECIFICALLY FOR THE PREPARATION OF BID PROPOSALS AND FOR CONSTRUCTION RELATED ACTIVITY. BY ACCEPTING THESE DOCUMENTS AND DRAWINGS, THE INDIVIDUAL, COMPANY OR AGENCY ACKNOWLEDGES THAT THESE DOCUMENTS AND DRAWINGS RELATE TO FACILITIES WHEREIN SENSITIVE AND SECURE GOVERNMENTAL ACTIVITIES ARE CONDUCTED. THE INDIVIDUAL'S, COMPANY'S OR AGENCY'S MISUSE OF THESE DOCUMENTS AND DRAWINGS MAY THREATEN THE SAFETY AND SECURITY OF GOVERNMENTAL OPERATIONS. BY ACCEPTING THESE DOCUMENTS AND DRAWINGS, THE INDIVIDUAL, COMPANY OR AGENCY, AGREES THAT IT SHALL RESTRICT DISSEMINATION OF SUCH MATERIALS TO RECOGNIZED, LEGITIMATE SUB-CONTRACTORS, SUPPLIERS AND MATERIAL MEANT SOLELY FOR THEIR USE IN PREPARATION OF QUOTES, BIDS OR PARTICIPATION IN ACTUAL CONSTRUCTION ACTIVITY. FURTHER, BY ACCEPTING THESE DOCUMENTS AND DRAWINGS, THE INDIVIDUAL, COMPANY OR AGENCY ASSUMES FULL RESPONSIBILITY FOR AND LIABILITY FOR SAID INDIVIDUAL'S, COMPANY'S OR AGENCY'S NEGLIGENCE AND FOR ITS CARELESS OR WRONGFUL DISTRIBUTION OF ERIE COUNTY PROJECT MANUALS, DRAWINGS AND CONSTRUCTION DOCUMENTS.

SPECIAL NOTE - RIGHT OF WAY:
 A. ALL WORK TO BE PERFORMED UNDER THIS CONTRACT WILL BE WITHIN THE PUBLIC RIGHT-OF-WAY (ROW) IN ACCORDANCE WITH SECTION 105-15 OF THE STANDARD SPECIFICATIONS. THE CONTRACTOR IS TO ASSURE HIMSELF THAT ALL WORK IS BEING PERFORMED WITHIN THE ROW, INCLUDING BUT NOT LIMITED TO VEHICLE ACCESS; STORAGE OF EQUIPMENT, MATERIALS, DEBRIS AND WASTE, LANDSCAPING, VEGETATION REMOVAL AND MANAGEMENT, GRADING, SEEDING AND THE INSTALLATION OF TURF; AND THE INSTALLATION OF ANY FENCES OR PROTECTIVE BARRIER.

B. IF CONTRACTOR IS UNABLE TO IDENTIFY THE LIMITS OF THE RIGHTS-OF-WAY WHEN THE CONTRACT CALLS FOR WORK IN THOSE VICINITIES, THE CONTRACTOR MUST CONTACT THE PROJECT ENGINEER FOR DEFINITIVE BOUNDARY DETERMINATIONS BEFORE ANY WORK MAY BE INITIATED AT THOSE LOCATIONS (STANDARD SPECIFICATIONS SECTIONS 105-10 AND 625).

C. IN ACCORDANCE WITH SECTION 105-15 OF THE STANDARD SPECIFICATIONS, RELEASES FOR ANY NON-ESSENTIAL CONTRACT WORK OUTSIDE OF THE EXISTING RIGHTS-OF-WAY, INCLUDING PLANTINGS, LANDSCAPING OR DRIVEWAY ENHANCEMENT, WILL BE PROVIDED BY THE PROJECT ENGINEER AND IN NO INSTANCE ARE TO BE SECURED BY THE CONTRACTOR. THE CONTRACTOR SHALL NOT INVADE UPON PRIVATE PROPERTIES, LANDS OR BUILDINGS OUTSIDE OF THE RIGHTS-OF-WAY FOR ANY REASONS WITHOUT FIRST SECURING WRITTEN PERMISSION FROM THE PROPERTY OWNER (STANDARD SPECIFICATIONS SECTION 105-15).

D. THE CONTRACTOR WILL BE HELD LIABLE FOR ANY DAMAGES DONE. ANY SUCH INJURIES OR DAMAGES SHALL BE SATISFACTORILY REPAIRED OR ITEMS REPLACED AT THE CONTRACTOR'S EXPENSE (STANDARD SPECIFICATIONS SECTION 107-08)

| | |
|--|---|
| Recommended for Approval Gina M. Wilkolaski, P.E. Project Manager County of Erie Department of Public Works Date: 5/13/26 | Recommended for Approval Darlene M. Svilokos, P.E. Director of Engineering County of Erie Department of Public Works Date: 05/13/26 |
| Approved By William R. Geary, Jr. Commissioner of Public Works County of Erie Department of Public Works Date: 13 May 2026 | |
| Erie County Department of Public Works - Division of Highways 93 Franklin Street - 14th Floor Buffalo, New York 14202 2026 OVERLAY B TITLE SHEET AND INDEX | |
| SHEET NO. 1 OF 41 DATE MAY 26 SCALE NONE | TITLE |

**CR 242 AND CR 294
2026 OVERLAY B PROJECT
PROJECT NO. CAP-242.294-26
SHEET 2 OF 41**

GENERAL NOTES:

- BOWEN ROAD WILL RECEIVE A COLD IN PLACE RECYCLE (CIPR) TREATMENT BY OTHERS. THE CONTRACTOR SHALL COORDINATE WITH ERIE COUNTY REGARDING THE TIMING OF THE TOP COURSE ASPHALT TO ENSURE THAT A MINIMUM OF 14 DAYS HAS PASSED SINCE THE CIPR, TO ALLOW THE RECYCLE TO CURE.
- NORTH FOREST ROAD PAVING SHALL BE COORDINATED WITH THE CONTRACTORS OF OTHER ROAD CONSTRUCTION PROJECTS IN THE VICINITY. THESE PROJECTS INCLUDE MAPLE ROAD PHASE 2 (AMHERST PAVING, INC), MAPLE ROAD BRIDGE REPAIRS (OAKGROVE CONSTRUCTION, INC.), AND NYS ROUTE 263 MILL AND RESURFACE (CATCO)
- EXCEPTION TO CURRENT NYSDOT STANDARD SPECIFICATION:
70 SERIES ASPHALT PAVEMENT WILL NOT REQUIRE USING THE DENSITY PROFILING SYSTEM (DPS) FOR THIS PROJECT. INSTEAD, THE DENSITY REQUIREMENTS WILL BE IN ACCORDANCE WITH SECTION 404-3.07 FROM NYSDOT STANDARD SPECIFICATION DATED SEPTEMBER 1, 2025, WHICH UTILIZES NUCLEAR DENSITY GAUGES FOR PAVEMENT MONITORING.
- THE CONTRACTOR SHALL NOT PARK HIS EQUIPMENT WHERE IT IS CONSIDERED BY THE ENGINEER TO BE A SAFETY HAZARD TO TRAFFIC. VEHICLES OR EQUIPMENT BELONGING TO THE CONTRACTOR OR HIS STAFF SHALL NOT BE PARKED ON ROADWAYS, SHOULDERS OR DITCHES ALONG ANY ROADWAY REMAINING OPEN TO TRAFFIC. EQUIPMENT PARKED OVERNIGHT ON THE R.O.W. SHALL BE PARKED BEHIND THE CONSTRUCTION BARRIERS.
- THE CONTRACTOR SHALL PROVIDE A SECURE STAGING AREA FOR STORAGE OF EQUIPMENT, MATERIALS, EMPLOYEE PARKING, AND OFFICE SPACE.
- THE RELOCATION OF MAILBOXES AND PAPER BOXES, AS MANY TIMES AS NEEDED, AND THE MAINTENANCE OF THE MAILBOXES AND PAPER BOXES IN SERVICEABLE CONDITION AND POSITION IS INCLUDED IN THIS WORK.
- PUBLIC STREETS, PRIVATE DRIVES AND PARKING FACILITIES SHALL BE KEPT FREE OF FOREIGN MATERIALS. ALL AREAS SHALL BE SWEEPED CLEAN AT THE END OF EACH WORKING DAY AND BY THE ENGINEER.
- WEEDS, SHRUBBERY, CONSTRUCTION MATERIALS, EQUIPMENT, VEHICLES, OR WORKERS SHALL NOT OBSCURE TRAFFIC CONTROL DEVICES OR OBSTRUCT TRAFFIC. THE CONTRACTOR SHALL BE REQUIRED TO TRIM ANY FOLIAGE OBSTRUCTING THE VISIBILITY OF ANY TRAFFIC CONTROL DEVICE WHETHER PERMANENT, TEMPORARY, OR CONSTRUCTION. THE COST OF THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR BASIC WORK ZONE TRAFFIC CONTROL, ITEM 619.01.
- DURING THE COURSE OF THIS PROJECT, THE CONTRACTOR SHALL KEEP THE TRAVEL WAY FREE OF BUMPS THAT MAY BE CREATED BY THE CONTRACTOR'S WORK OPERATIONS. A BUMP IS DEFINED AS A PAVEMENT CONDITION CONSIDERED BY THE ENGINEER TO BE SUFFICIENTLY ABRUPT TO CAUSE CONSIDERABLE DISCOMFORT, CARGO SHIFTING, OR DEFLECTION OF A VEHICLE FROM ITS TRUE COURSE AT PREVAILING DRIVING SPEEDS. THE CONTRACTOR SHALL PROVIDE TEMPORARY RAMPS FOR ALL BUMPS CREATED BY THE CONTRACTOR'S WORK OPERATIONS, INCLUDING MILLING AND PAVING. THE CONTRACTOR IS RESPONSIBLE FOR PLACEMENT AND REMOVAL OF THESE RAMPS. TEMPORARY RAMPS SHALL BE CONSTRUCTED FROM GRANULAR PATCHING MATERIAL (PLANT MIX OR A SUITABLE WINTER MIX) SO AS TO CREATE THE SMOOTHEST TRANSITION POSSIBLE (60:1, OR AS DETERMINED BY THE ENGINEER). THESE RAMPS SHALL BE MAINTAINED AND REMAIN IN PLACE UNTIL THE NEED FOR RAMPING IS ELIMINATED BY SUBSEQUENT WORK OPERATIONS. THE COST BID FOR THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR BASIC WORK ZONE TRAFFIC CONTROL, ITEM 619.01.
- A W8-1 "BUMP" SIGN SHALL BE PLACED ON ALL APPROACHES WHERE A BUMP IS PRESENT IN THE ROADWAY.
- NYSDOT NORTH ERIE RESIDENCY (716-684-6180) MUST BE NOTIFIED 48 HOURS PRIOR TO ALL MILLING AND PAVING OPERATIONS.
- THE CONTRACTOR SHALL CLOSELY COORDINATE ALL WORK IMPACTING SCHOOL PEDESTRIAN TRAFFIC AND SCHOOL BUS TRAFFIC WITH THE RESIDENT ENGINEER OR SPECIFIED AGENT. SAFE SCHOOL ROUTE MAINTENANCE PLANS SHALL BE SUBMITTED AND APPROVED PRIOR TO BEGINNING ANY WORK WHICH IMPACTS THE ROUTE. ANY ADDITIONAL SIGNAGE, FLAGGERS, OR OTHER REQUIREMENTS ORDERED BY THE RESIDENT ENGINEER SHALL BE INCLUDED IN THE UNIT PRICE BID FOR BASIC WORK ZONE TRAFFIC CONTROL. THE SAFE SCHOOL ROUTE MAINTENANCE PLAN SHALL STAY IN EFFECT FOR THE DURATION OF CONSTRUCTION OR A.O.B.E.
- IN ADDITION TO THE NYSDOT DROP-OFF POLICY OF SECTION 619-3.02 OF THE NYSDOT STANDARD SPECIFICATIONS, NO OVERNIGHT LANE DROPOFFS IN EXCESS OF 2 INCHES SHALL BE ALLOWED.
- THE CONTRACTOR SHALL PROVIDE A STABLE, SUITABLE DRIVING SURFACE FOR THE PUBLIC AT ALL TIMES.
- NO NIGHT WORK WILL BE ALLOWED UNLESS PRIOR APPROVAL IS GIVEN BY THE TOWN AND ERIE COUNTY PRIOR TO THE START OF THE PROJECT. ADDITIONAL WORK ZONE TRAFFIC CONTROL WILL BE REQUIRED INCLUDING THE ADDITION OF REFLECTIVE MATERIALS AND LIGHTING, AT NO ADDITIONAL COST.
- AREAS BEYOND THE WORK LIMITS DISTURBED, FOR ANY REASON, BY THE CONTRACTOR AND HIS EMPLOYEES SHALL BE RESTORED TO THE SATISFACTION OF THE ENGINEER AT THE CONTRACTOR'S EXPENSE.
- THE CONTRACTOR'S ATTENTION IS DIRECTED TO THE FACT THAT QUANTITIES AND FIELD CONDITIONS IDENTIFIED ON THESE PLANS MAY CHANGE DURING CONSTRUCTION. DUE TO THE NATURE OF RECONSTRUCTION PROJECTS, THE EXACT EXTENT OF RECONSTRUCTION WORK CANNOT ALWAYS BE ACCURATELY DETERMINED PRIOR TO THE COMMENCEMENT OF WORK. THESE CONTRACT DOCUMENTS HAVE BEEN PREPARED BASED ON SURVEY, FIELD INSPECTION AND OTHER AVAILABLE INFORMATION. ACTUAL FIELD CONDITIONS MAY REQUIRE MODIFICATIONS TO CONSTRUCTION DETAILS AND WORK QUANTITIES. THE CONTRACTOR SHALL PERFORM THE WORK IN ACCORDANCE WITH FIELD CONDITIONS.
- THE CONTRACTOR SHALL EXAMINE AND VERIFY ALL CONDITIONS AND DIMENSIONS PRIOR TO BIDDING. IF FIELD CONDITIONS AND DIMENSIONS DIFFER FROM THOSE SHOWN ON THE PLANS, THE CONTRACTOR SHALL USE THE FIELD CONDITIONS AND DIMENSIONS AND MAKE THE APPROPRIATE CHANGES TO THOSE SHOWN ON THE PLANS AS APPROVED BY THE ENGINEER. THE RESULTS OF THIS CHECK OF CONDITIONS AND DIMENSIONS SHALL BE SO NOTED ON THE DRAWINGS SUBMITTED FOR APPROVAL. NO EXTRA COMPENSATION WILL BE ALLOWED TO THE CONTRACTOR BECAUSE OF THE CONTRACTOR'S FAILURE TO INCLUDE IN HIS BID ALL ITEMS AND MATERIALS WHICH HE IS REQUIRED TO FURNISH IN ACCORDANCE WITH THE CONTRACT DOCUMENTS.
- ALL SHOP DRAWINGS SUBMITTED FOR THIS PROJECT SHALL BE IN ENGLISH UNITS.
- THE CONTRACTOR SHALL SUBMIT A DETAILED CONSTRUCTION SCHEDULE TO THE ENGINEER AT THE PRE-CONSTRUCTION MEETING FOR APPROVAL. THE CONTRACTOR SHALL NOT PROCEED WITH ANY WORK PRIOR TO RECEIVING WRITTEN APPROVAL OF THE CONSTRUCTION SCHEDULE.
- THE CONTRACTOR SHALL LOCATE ALL PERTINENT PROPERTY, EASEMENT AND/OR RIGHT-OF-WAY LINES PRIOR TO COMMENCEMENT OF THE WORK. THE CONTRACTOR SHALL SAFEGUARD AND PRESERVE ALL SURVEY CONTROL MONUMENTS, RIGHT-OF-WAY MONUMENTS AND PROPERTY PINS WITHIN THE PROJECT AREA. THE CONTRACTOR SHALL NOT TRESPASS OR COMMENCE WORK ON OTHER PROPERTIES, EASEMENTS, OR RIGHT OF WAYS WITHOUT PROPERLY NOTIFYING AND MEETING THE NECESSARY REQUIREMENTS OF THE PROPERTY OWNER OR AUTHORIZED AGENCY.

EMERGENCY CONTACT NOTES:

- ERIE COUNTY WILL CONTACT ALL SCHOOLS AND EMERGENCY RESPONSE AGENCIES THAT MAY BE AFFECTED BY WORK ASSOCIATED WITH THIS PROJECT AND WILL NOTIFY THEM OF ANTICIPATED CHANGES IN TRAVEL CONDITIONS, TO FACILITATE THIS, THE CONTRACTOR MUST NOTIFY THE COUNTY EACH TIME PROJECT ACTIVITIES CHANGE AND NEW TRAVEL CONDITIONS ARE CREATED. NOTIFICATIONS AND UPDATES SHALL BE PROVIDED AT LEAST TWO WEEKS PRIOR TO THE ALTERATION OF TRAVEL CONDITIONS, OR CHANGE IN SCHEDULE.
- THE CONTRACTOR SHALL COORDINATE ANY WORK WITH OTHER CONTRACTORS, UTILITY COMPANIES, MUNICIPALITIES, AND PERMITTEES IN ACCORDANCE WITH STANDARD SPECIFICATION 105-06, TO ENSURE ALL TRAFFIC IS PROPERLY AND SAFELY MAINTAINED.
- AT THE PRE-CONSTRUCTION MEETING, THE CONTRACTOR SHALL SUBMIT AN EMERGENCY CONTACT LISTING TO THE COUNTY, PROVIDING THE NAME, ADDRESS AND TELEPHONE NUMBER OF PERSONS AUTHORIZED TO SECURE EQUIPMENT AND LABOR OUTSIDE OF NORMAL WORK HOURS.

EMERGENCY ACCESS NOTES:

- AT ALL TIMES DURING CONSTRUCTION, THE CONTRACTOR MUST PROVIDE SAFE AND CONVENIENT EMERGENCY ACCESS.

TIME AND HOLIDAY RESTRICTION NOTES:

- NO WORK/SHOULDER CLOSURE/LANE CLOSURE WILL BE ALLOWED AT MILLERSPORT HIGHWAY INTERSECTION FROM 6AM TO 9AM AND FROM 3PM TO 6PM.
- NO WORK/SHOULDER CLOSURE/LANE CLOSURE WILL BE ALLOWED FROM NOON FRIDAY UNTIL TUESDAY, ON THE FOLLOWING 2026 OBSERVED HOLIDAY WEEKENDS: VICTORIA DAY (MONDAY, MAY 18), MEMORIAL DAY (MONDAY, MAY 25), LABOR DAY (MONDAY, SEPTEMBER 7), AND CANADA CIVIC HOLIDAY (MONDAY, AUGUST 3).
- NO WORK/SHOULDER CLOSURE/LANE CLOSURE WILL BE ALLOWED FROM NOON THE DAY BEFORE UNTIL NOON THE DAY AFTER FOR CANADA DAY (TUESDAY, JULY 1), INDEPENDENCE DAY (SATURDAY, JULY 4), AND CHRISTMAS DAY (FRIDAY, DECEMBER 25) AND FROM NOON WEDNESDAY BEFORE UNTIL NOON THE MONDAY AFTER FOR THANKSGIVING DAY (WEDNESDAY, NOVEMBER 26).
- TIME RESTRICTIONS ARE FOR ANY WORKDAY, SUNDAY THROUGH SATURDAY, INCLUSIVE. ANY WAIVER OF THESE RESTRICTIONS NEEDS TO BE APPROVED BY THE DIRECTOR.
- ROADWORK IS DEFINED AS ANY LANE AND/OR SHOULDER WORK THAT REQUIRES CLOSURE.

MAINTENANCE AND PROTECTION OF TRAFFIC NOTES:

- THE CONTRACTOR SHALL MAINTAIN THE TRAFFIC CONTROL THROUGHOUT THE LENGTH OF THE CONTRACT IN ACCORDANCE WITH THE REQUIREMENTS OF SECTION 619 OF THE NYSDOT STANDARD SPECIFICATIONS DATED MAY 1, 2026, THE NATIONAL MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (M.U.T.C.D.) AND THE NYS SUPPLEMENTAL MANUAL OF TRAFFIC CONTROL DEVICES (NY M.U.T.C.D.), THE MAINTENANCE AND PROTECTION OF TRAFFIC DETAILS IN THE PLANS AND PROPOSAL OF THIS CONTRACT, AND AS ORDERED BY THE ENGINEER OR ERIE COUNTY (A.O.B.E.).
- MAINTENANCE AND PROTECTION OF TRAFFIC SHALL BE PROVIDED THROUGHOUT THE LENGTH OF THE CONTRACT IN ACCORDANCE WITH THE PROVISIONS OF SECTION 619 OF THE NYSDOT STANDARD SPECIFICATIONS AND STANDARD SHEETS FOR WORK ZONE TRAFFIC CONTROL. THE MANUAL ON TRAFFIC CONTROL DEVICES AND ANY PROVISIONS CONTAINED IN THE CONTRACT DOCUMENTS, AND AS ORDERED BY THE ENGINEER (A.O.B.E.), THE MANUAL ON TRAFFIC CONTROL DEVICES SHALL BE DEFINED AS THE MUTCD, 2023 11th EDITION AND THE NYS SUPPLEMENT 2010 EDITION.
- AS SUGGESTED IN THE WORK ZONE TRAFFIC CONTROL TABLE (STANDARD SHEET 619), THE CONTRACTOR SHALL BE RESPONSIBLE FOR UTILIZING ALL APPLICABLE NYSDOT STANDARD SHEETS, AND ALL PROVISIONS OF SECTION 619, FOR ALL WORK ZONE TRAFFIC CONTROL. THIS INCLUDES THE USE OF NOTES, TABLES AND DETAILS WITHIN SECTION 619, THE TYPICAL DETAILS DEPICTED ON THE NYSDOT STANDARD SHEETS, NYSDOT WORK ZONE TRAFFIC CONTROL MANUAL AND IN THE MUTCD, REFLECT THE MINIMUM REQUIREMENTS.
- THE TRAFFIC CONTROL PLANS SHOWN ARE TO BE CONSIDERED MINIMUM REQUIREMENTS. ADDITIONAL SIGNS AND/OR TRAFFIC CONTROL DEVICES MAY BE REQUIRED AS DETERMINED BY THE ENGINEER. COST TO BE INCLUDED IN THE PRICE BID FOR ITEM 619.01. IF AT ANY TIME THE ENGINEER DETERMINES THAT TRAFFIC IS NOT BEING PROPERLY MAINTAINED WITHIN OR AROUND A WORK ZONE, THE CONTRACTOR SHALL IMMEDIATELY CORRECT THE INDICATED DEFICIENCY.
- THE CONTRACTOR MAY SUBMIT REVISIONS TO THE MAINTENANCE AND PROTECTION OF TRAFFIC PLANS FOR APPROVAL. ANY CHANGES THAT ALTER THE BASIC CONCEPTS OF THE PLANS MUST BE APPROVED BY THE ENGINEER IN CHARGE AND ERIE COUNTY.
- THE MINIMUM LANE WIDTHS FOR WORK ZONE TRAVEL LANES SHALL BE 10'.
- THE CONTRACTOR SHALL MAINTAIN A MINIMUM OF 500' LONGITUDINAL DISTANCE BETWEEN CONSTRUCTION OPERATIONS ON ALTERNATE SIDES OF THE ROADWAY, UNLESS OTHERWISE APPROVED BY THE ENGINEER.
- WHEN TWO OR MORE AREAS ARE ADJACENT, OVERLAP, OR ARE IN CLOSE PROXIMITY, THE CONTRACTOR SHALL ENSURE THERE ARE NO CONFLICTING SIGNS AND THAT LANE CONTINUITY IS MAINTAINED THROUGHOUT ALL WORK AREAS.
- WHEN A DRIVEWAY INTERSECTS THE ROADWAY WITHIN A WORK ZONE TRAFFIC CONTROL AREA, ADDITIONAL TEMPORARY TRAFFIC CONTROL DEVICES AND/OR FLAGGERS SHALL BE PLACED AS NEEDED. ADDITIONAL FLAGGERS SHALL BE LOCATED AT ALL INTERSECTIONS AND COMMERCIAL DRIVEWAYS LOCATED WITHIN OR NEAR THE ACTIVE WORK SPACE.
- DURING NON-WORKING HOURS THE CONTRACTOR SHALL DELINEATE THE CENTERLINE AND BOTH EDGES OF TRAVEL LANES WITH DELINEATION DEVICES.
- THE CONTRACTOR SHALL MAINTAIN CENTERLINE DELINEATION UNTIL SHORT TERM PAVEMENT MARKING CAN BE PLACED ON THE BINDER COURSE OR FINAL CENTERLINE MARKINGS ARE INSTALLED. THE COST OF THIS WORK SHALL BE INCLUDED IN THE PRICE BID ITEM 619.01, BASIC WORK ZONE TRAFFIC CONTROL.

PUBLIC ACCESS NOTES:

- PROPERTY OWNERS WHOSE DRIVEWAYS WILL BE MADE INACCESSIBLE SHALL BE NOTIFIED BY THE CONTRACTOR AT LEAST 24 HOURS PRIOR TO RESTRICTING USE OF THE DRIVEWAY. FOR MULTIPLE ACCESS PROPERTIES, AT LEAST ONE DRIVEWAY SHALL BE OPEN AT ALL TIMES. ACCESS SHALL BE RESTORED TO ALL DRIVEWAYS AS SOON AS POSSIBLE.
- RIGHT-OF-WAY SPECIAL NOTES:
1. ALL WORK TO BE PERFORMED UNDER THIS CONTRACT WILL BE WITHIN THE PUBLIC RIGHT-OF-WAY (ROW) IN ACCORDANCE WITH SECTION 105-15 OF THE STANDARD SPECIFICATIONS. THE CONTRACTOR IS TO ASSURE HIMSELF THAT ALL WORK IS BEING PERFORMED WITHIN THE ROW, INCLUDING BUT NOT LIMITED TO VEHICLE ACCESS; STORAGE OF EQUIPMENT, MATERIALS, DEBRIS AND WASTE; LANDSCAPING; VEGETATION REMOVAL AND MANAGEMENT; GRADING, SEEDING AND THE INSTALLATION OF TURF; AND THE INSTALLATION OF ANY FENCES OR PROTECTIVE BARRIER.
- IN ACCORDANCE WITH SECTION 105-15 OF THE STANDARD SPECIFICATIONS, RELEASES FOR ANY NON-ESSENTIAL CONTRACT WORK OUTSIDE OF THE EXISTING RIGHTS-OF-WAY, INCLUDING PLANTINGS, LANDSCAPING OR DRIVEWAY ENHANCEMENT, WILL BE PROVIDED BY THE PROJECT ENGINEER AND IN NO INSTANCE ARE TO BE SECURED BY THE CONTRACTOR. THE CONTRACTOR SHALL NOT INVADE UPON PRIVATE PROPERTIES, LANDS OR BUILDINGS OUTSIDE OF THE RIGHTS-OF-WAY FOR ANY REASON WITHOUT FIRST SECURING WRITTEN PERMISSION FROM THE PROPERTY OWNER (STANDARD SPECIFICATIONS SECTION 105-15).
- THE CONTRACTOR WILL BE HELD LIABLE FOR ANY DAMAGES DONE. ANY SUCH INJURIES OR DAMAGES SHALL BE SATISFACTORILY REPAIRED OR ITEMS REPLACED AT THE CONTRACTOR'S EXPENSE (STANDARD SPECIFICATIONS SECTION 107-08).

PORTABLE VARIABLE MESSAGE SIGN NOTES:

- PORTABLE VARIABLE MESSAGE SIGNS SHALL BE PLACED BEYOND THE SHOULDER WHEREVER PRACTICAL.
- PORTABLE VARIABLE MESSAGE SIGNS SHALL REFLECT CURRENT WORK ZONE CONDITIONS AND SHALL BE UPDATED WHEN CONDITIONS CHANGE. PORTABLE VARIABLE MESSAGE SIGNS SHALL BE TURNED OFF AT TIMES WHEN A MESSAGE IS NOT NEEDED. MESSAGES SHALL NOT REPEAT INFORMATION ALREADY CONVEYED BY STATIC WARNING SIGNS.
- PORTABLE VARIABLE MESSAGE SIGNS SHALL BE USED ONE WEEK IN ADVANCE OF ANY WZTC SET-UP.
- PVMS: LOCATED 2,000 FT. IN ADVANCE OF THE PROJECT LIMITS.
- BEFORE CONSTRUCTION: RIGHT LANE CLOSE AHEAD 1 DAY DATE TIME.
- DURING CONSTRUCTION: RIGHT LANE CLOSED USE CAUTION.

PEDESTRIAN NOTES:

- PEDESTRIAN TRAFFIC SHALL BE MAINTAINED AT ALL TIMES DURING CONSTRUCTION. THE CONTRACTOR SHALL FOLLOW THE STANDARD SHEETS TO CROSS PEDESTRIANS AT THE CLOSEST CROSSWALK POSSIBLE.
- CONTRACTOR TO CONTACT NYSDOT LANDSCAPE ENVIRONMENTAL GROUP 5 WORKING DAYS PRIOR TO SIDEWALK/HANDICAP RAMP AND CURB WORK TO ENSURE ADA COMPLIANCE
- THE CONTRACTOR SHALL PROVIDE, DELINEATE AND MAINTAIN A USABLE PATH OF TRAVEL FOR PEDESTRIANS EITHER THROUGH OR AROUND WORK ZONES FOR THE DURATION OF THIS PROJECT. IN ACCORDANCE WITH THE PLANS, THE PATH OF TRAVEL SHALL COMPLY WITH THE "CONTINUOUS PASSAGE" PROVISIONS OF THE CURRENT AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES (ADAAG) AND THE 2024 ADOPTED ACCESSIBILITY GUIDELINES FOR PEDESTRIAN FACILITIES IN THE PUBLIC RIGHT OF WAY (PROWAG). THE CONTRACTOR MAY UTILIZE ANY COMBINATION OF THE FOLLOWING TO PROVIDE THE USABLE PATH OF TRAVEL: EXISTING SIDEWALKS/SHOULDERS; TEMPORARY SIDEWALKS/SHOULDERS; FINAL SIDEWALKS/SHOULDERS; AND EXISTING OR TEMPORARY SIDEWALK CURB RAMPS. THE SIDEWALK TRAVEL PATH SHALL BE CLEARLY MARKED AND STAGED BETWEEN INTERSECTING ROADS TO ENSURE SAFE PEDESTRIAN CROSSING AND ACCESSIBILITY. PEDESTRIAN ACCESS TO RESIDENTIAL AND COMMERCIAL PROPERTIES SHALL BE MAINTAINED FOR THE DURATION OF CONSTRUCTION.
- THE CONTRACTOR SHALL CONDUCT WORK OPERATIONS TO MINIMIZE THE INTERRUPTION OF PEDESTRIAN TRAFFIC. THE TEMPORARY STORAGE OF CONSTRUCTION EQUIPMENT OR MATERIALS SHALL NOT BE PERMITTED ON ANY USABLE TRAVEL PATH THAT IS OPEN TO PEDESTRIAN TRAFFIC.
- ALL LABOR, MATERIALS AND EQUIPMENT NECESSARY TO INSTALL AND REMOVE TEMPORARY SIDEWALK AND CURB RAMPS, INCLUDING THE RESTORATION OF DISTURBED AREAS, SHALL BE INCLUDED IN THE PRICE BID FOR BASIC WORK ZONE TRAFFIC CONTROL, ITEM 619.01.

CONSTRUCTION AND TRAFFIC SIGNAGE NOTES:

- ALL WZTC SIGNS SHALL BE PLACED WITHIN THE RIGHT OF WAY.
- ALL SIGNS SHALL REFLECT ACTUAL CONDITIONS AT ALL TIMES AND SHALL BE COVERED, REMOVED, RELOCATED OR CHANGED IMMEDIATELY AS CONDITIONS REQUIRE.
- THE MODIFICATION, RELOCATION OR ADJUSTMENT OF EXISTING SIGNS SHALL BE AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER, AND SHALL BE INCLUDED IN ITEM 619.01.
- SIGN LOCATIONS SHOWN ARE APPROXIMATE ONLY. EXACT LOCATIONS SHALL BE DETERMINED BY THE CONTRACTOR AND APPROVED VERBALLY BY THE COUNTY. TAKE CARE NOT TO OBSCURE OTHER SIGNS, PARTICULARLY STOP SIGNS. AVOID PLACING SIGNS IMMEDIATELY IN FRONT OF HOUSES. AVOID PLACING SIGNS WHERE SITE DISTANCE IS OBSCURED. THE SIGNAGE AND BARRICADES SHALL BE MODIFIED AS NEEDED AS THE WORK PROGRESSES.
- THE CONTRACTOR SHALL ENSURE THAT PLACEMENT OF CONES, DRUMS, OR BARRICADES WILL NOT INTERFERE WITH SIGHT DISTANCE.
- WHERE POSSIBLE ALL CHANNELIZATION DEVICES ARE TO BE PLACED SO AS TO PROVIDE A MINIMUM 2' LATERAL CLEARANCE TO THE TRAVELED WAY.
- ALL SIGNS SHALL BE SIZE "C". THE CONTRACTOR MAY PLACE AS MANY AS SIX (6) ADDITIONAL SIZE "C" SIGNS A.O.B.E.
- PRIOR TO THE START OF ANY WORK OPERATIONS, ALL RELATED WORK FOR PROPOSED WORK ZONE TRAFFIC CONTROL SHALL BE COMPLETE. THIS INCLUDES BUT IS NOT LIMITED TO, ALL SIGNS, SIGNALS, PAVEMENT MARKINGS, BARRIERS, DELINEATION (CONES, DRUMS, ETC.), FLAGGERS, PAVEMENT MODIFICATIONS, AND ANY OTHER RELATED WORK.
- PRIOR TO AND THROUGHOUT CONSTRUCTION, THE CONTRACTOR SHALL POST SIGNAGE IN CONFORMANCE WITH THE REQUIREMENTS OF ERIE COUNTY AND OCCUPATIONAL SAFETY AND HEALTH ADMINISTRATION (OSHA), JOB SAFETY AND MAINTENANCE AND PROTECTION OF TRAFFIC IS THE RESPONSIBILITY OF THE CONTRACTOR. THE CONTRACTOR SHALL MAINTAIN ALL FENCING AND BARRICADES THROUGHOUT THE DURATION OF CONSTRUCTION. THE PROJECT SITE FENCING SHALL BE SECURE DURING OFF-WORK HOURS TO HELP ENSURE PUBLIC SAFETY. THE CONTRACTOR SHALL COORDINATE ANY REMOVAL OF EXISTING FENCING AND BARRICADES (PROPERTY OF E.C.D.P.W.) WITH E.C.D.P.W.
- THE TYPE II CONSTRUCTION BARRICADES AND TEMPORARY CONCRETE BARRIER SHALL BE SUPPLIED WITH TYPE B LIGHTING WHICH SHALL FUNCTION BOTH DAY AND NIGHT. ALL CLOSURE SIGNS THAT INDICATE THAT FLASHING LIGHTS ARE REQUIRED SHALL BE SUPPLIED WITH TYPE A LIGHTING THAT OPERATES ONLY AT NIGHT, THE COST OF MAINTAINING ALL LIGHTING SHALL BE INCLUDED IN THE RESPECTIVE ITEM FOR WHICH THE LIGHTING IS ATTACHED.
- ALL BARRICADES SHALL BE FULLY LIGHTED. COST OF LIGHTS INCLUDED IN ITEM 619.04.
- LIGHTING FOR BARRICADES SHALL BE HIGH INTENSITY FOR FULL BARRICADES AND LOW INTENSITY FOR PARTIAL BARRICADES.
- PLACE SIGN BARRICADE AT SHOULDER WITH WARNING LIGHTS - ITEM 619.04.
- IF AT ANY TIME THE ENGINEER DETERMINES THAT TRAFFIC IS NOT BEING PROPERLY MAINTAINED WITHIN A WORK ZONE, THE CONTRACTOR SHALL IMMEDIATELY CORRECT THE INDICATED DEFICIENCY AS DIRECTED, TO THE SATISFACTION OF THE ENGINEER.
- DELINEATION DEVICES SHALL CONFORM TO THE NATIONAL M.U.T.C.D. AND THE NYS SUPPLEMENT.
- THE CONTRACTOR SHALL NOT MIX DELINEATION DEVICES ON A LINEAR CLOSURE OR TAPER (I.E. CONES, VERTICAL PANELS, TUBULAR MARKERS, OR DRUM SHALL NOT BE USED IN THE SAME TAPER OR CLOSURE); HOWEVER, DIFFERENT DELINEATION DEVICES MAY BE USED IN DIFFERENT AREAS OF THE PROJECT.
- ACCESS TO PRIVATE AND COMMERCIAL DRIVEWAYS INCLUDING FARM FIELD DRIVEWAYS SHALL BE MAINTAINED AT ALL TIMES DURING THE LIFE OF THE CONTRACT. TEMPORARY DRIVEWAYS SHALL BE PROVIDED AS NECESSARY. THE CONTRACTOR SHALL NOT PARK ANY EQUIPMENT WHERE IT IS CONSIDERED BY THE COUNTY TO BE A SAFETY HAZARD TO TRAFFIC. VEHICLES OR EQUIPMENT BELONGING TO THE CONTRACTOR OR WORKERS SHALL NOT BE PARKED ON TRAVELED WAY, SHOULDER, TURF SHOULDER, DITCH FRONT SLOPE, DITCH BOTTOM, OR THE BOTTOM PORTION OF THE DITCH BACK SLOPE ALONG A ROADWAY BEING USED BY THE GENERAL PUBLIC.
- SIGNS INADVERTENTLY REMOVED BY THE CONTRACTOR AND NOT SPECIFIED ON THESE PLANS ARE TO BE REPLACED BY THE CONTRACTOR AT NO COST TO ERIE COUNTY.

PROJECT STAGING NOTES:

- DURING WORKING HOURS, THE CONTRACTOR SHALL MAINTAIN TWO-WAY TRAFFIC THROUGH THE PROJECT WORK ZONE AT ALL TIMES.
- ANY CHANGES TO THIS TRAFFIC PLAN SHALL BE SUBMITTED TO THE COUNTY FOR REVIEW AT LEAST 2 WEEKS PRIOR TO THE START OF WORK.
- TO MAINTAIN TWO LANES OF TRAFFIC, WHEN THE CONTRACTOR IS NOT WORKING, ANY OPEN EXCAVATIONS WITHIN THE ROADWAY SHALL BE COVERED WITH METAL PLATES OR OTHERWISE DELINEATED, A.O.B.E.

EROSION CONTROL NOTES:

- THE CONTRACTOR IS TO TAKE APPROPRIATE MEASURES TO PREVENT DIRT, DEBRIS AND CONSTRUCTION MATERIALS FROM ENTERING THE SEWER MAIN, LATERALS, CATCH BASINS, MANHOLES, SWALES, BIOTRETMENT SITES, DRAINAGE INLETS, AND OTHER APPURTENANCES DURING CONSTRUCTION. NO SEPARATE PAYMENT UNLESS OTHERWISE NOTED.
- ONLY AREAS NEEDED FOR CONSTRUCTION AS SHOWN ON THESE PLANS SHALL BE DISTURBED. NO OTHER TREES, SHRUBS, OR GRASSES ARE TO BE REMOVED FOR CONSTRUCTION. REMAINING VEGETATION SHALL BE PROTECTED TO PRESERVE THEIR AESTHETIC AND EROSION AND SEDIMENT CONTROL VALUES.

FLAGGING NOTES:

- FLAGGER STATIONS SHALL BE ADJACENT TO THE TRAFFIC LANES BEING CONTROLLED AND SHALL BE POSITIONED SO THAT FLAGGER VISIBILITY TO APPROACHING TRAFFIC IS OPTIMIZED. THE FLAGGER SHALL BE READILY VISIBLE TO APPROACHING TRAFFIC WELL IN EXCESS OF THE REQUIRED STOPPING SIGHT DISTANCE FOR THE PREVAILING APPROACH ACCORDINGLY, THE TANGENT DIMENSIONS ON EACH END OF THE WORK AREA SHALL BE ESTABLISHED TO ACHIEVE THE APPROPRIATE FLAGGER STATION LOCATION, A.O.B.E.
 - FLAGGERS SHALL BE REQUIRED TO USE TWO-WAY RADIOS, WALKIETALKIE OR OTHER FORMS OF ENHANCED COMMUNICATION WHEN ONE FLAGGER IS NOT VISIBLE TO THE OTHER, OR IF THE ENGINEER DEEMS IT NECESSARY.
 - STOP/SLOW PADDLES SHALL BE USED BY ALL FLAGGERS AND SHALL CONFORM TO THE REQUIREMENTS SET FORTH IN THE MUTCD. FLAGGER SHALL USE HAND SIGNALS IN CONJUNCTION WITH THE PADDLES AS SHOWN IN THE MUTCD.
 - ALL LABOR, MATERIALS AND EQUIPMENT ASSOCIATED WITH FLAGGING OPERATIONS SHALL BE INCLUDED IN THE PRICE BID FOR ITEM 619.01.
 - W20-7A "FLAGGER" SIGNS SHALL BE USED WHENEVER FLAGGING OCCURS FOR MORE THAN A BRIEF PERIOD OF TIME (15 MINUTE MAX.). THE SIGN SHALL BE PROMPTLY REMOVED, COVERED OR TURNED AWAY FROM TRAFFIC WHEN THE FLAGGING OPERATION CEASES.
 - AT EACH SIDE ROAD INTERSECTION WITHIN THE TEMPORARY TRAFFIC CONTROL ZONE, POST A W20-4F "ONE LANE ROAD AHEAD", W8-22E "FLAGGER" SIGN AND FLAGGER. ADDITIONAL FLAGGERS SHALL BE POSTED TO CONTROL DRIVEWAY TRAFFIC ENTERING THE TEMPORARY TRAFFIC CONTROL ZONE AS DIRECTED BY THE PROJECT MANAGER. THE COST OF FLAGGERS SHALL BE INCLUDED IN THE PRICE BID FOR WORK ZONE TRAFFIC CONTROL.
- UTILITY NOTES:**
- THE CONTRACTOR SHALL CONFIRM THE DEPTH AND LOCATION OF ALL UNDERGROUND UTILITIES PRIOR TO CONSTRUCTION.
 - THE CONTRACTOR SHALL MAINTAIN SERVICE OF ALL EXISTING UTILITIES DURING CONSTRUCTION. THE CONTRACTOR SHALL BE FULLY RESPONSIBLE FOR ANY DISRUPTION OF UTILITY SERVICE, DAMAGE TO EXISTING SITE FEATURES, INCLUDING UTILITY APPURTENANCES, RESULTING FROM THE CONTRACTOR'S EXECUTION OF THE WORK SHALL BE RESTORED BY THE CONTRACTOR TO THE OWNER'S SATISFACTION OR A.O.B.E., AT NO ADDITIONAL COST TO ERIE COUNTY.
 - IN THE EVENT THE CONTRACTOR DAMAGES AN EXISTING UTILITY SERVICE, CAUSING AN INTERRUPTION IN SAID SERVICE, HE SHALL IMMEDIATELY COMMENCE WORK TO RESTORE SERVICE AND MAY NOT CEASE SUCH WORK OPERATION UNTIL SERVICE IS RESTORED.
 - NOTIFY THE ONE CALL CENTER (DIG NY) IN ACCORDANCE WITH REGULATIONS, TYPICALLY 3 TO 10 DAYS PRIOR TO STARTING ANY WORK THAT MIGHT BE CONSIDERED EXCAVATION OR DEMOLITION. POSTPONING THE JOB WILL LIKELY REQUIRE RE-NOTIFICATION TO THE ONE CALL CENTER. FURTHERMORE, IF THE JOB SITE BECOMES INACTIVE, CALL TO HAVE THE SPECIFIC WORK AREA REMARKED PRIOR TO RESTARTING THE WORK.
 - DESIGNATING THE DIG SITE IN WHITE WILL AID IN THE TIMELY MARKING OF UNDERGROUND FACILITIES. PRE-MARKING PROPOSED FACILITIES IN WHITE IS REQUIRED IN SOME CASES.
 - AFTER COMMENCEMENT OF EXCAVATION OR DEMOLITION WORK, THE EXCAVATOR IS RESPONSIBLE FOR PROTECTING AND PRESERVING THE STAKING, OR MARKING UNTIL EXCAVATION OR DEMOLITION WORK IN THE AREA IS COMPLETE. IF NECESSARY, CALL THE ONE CALL CENTER TO HAVE THE LINES REMARKED.
 - WHERE AN UNDERGROUND FACILITY HAS BEEN STAKED, MARKED OR OTHERWISE DESIGNATED, VERIFY THE PRECISE LOCATION, TYPE, SIZE, DIRECTION OF RUN AND DEPTH OF THE UNDERGROUND FACILITY. USE PRUDENT TECHNIQUES SUCH AS VACUUM EXCAVATION OR HAND-DUG TEST HOLES, TO VERIFY THE PRECISE LOCATION OF FACILITIES. IT IS BEST TO VERIFY PRIOR TO EXCAVATING IN THE GENERAL AREA, BUT VERIFICATION MUST BE DONE PRIOR TO EXCAVATING IN THE TOLERANCE ZONE. CALL THE FACILITY OWNER IF A MARKED FACILITY CANNOT BE FOUND.
 - INFORM EACH EQUIPMENT OPERATOR OF THE INFORMATION OBTAINED FROM FACILITY OWNERS. PLAN THE EXCAVATION OR DEMOLITION TO AVOID DAMAGE TO OR MINIMIZE INTERFERENCE WITH A FACILITY OWNER'S FACILITIES IN THE CONSTRUCTION AREA.
 - CONSULT THE FACILITY OWNER IF:
A. EXCAVATION OR DEMOLITION WORK REQUIRES TEMPORARY OR PERMANENT INTERRUPTION OF A FACILITY OWNER'S SERVICE. SUCH INTERRUPTION MUST BE COORDINATED WITH THE AFFECTED FACILITY OWNER IN ALL CASES.
B. A FACILITY IS EXPOSED OR UNDERMINED. THE EXCAVATOR MUST PROVIDE SUPPORT AND MECHANICAL PROTECTION FOR FACILITY OWNER'S LINES AT THE CONSTRUCTION SITE DURING EXCAVATION OR DEMOLITION WORK, INCLUDING DURING BACKFILLING OPERATIONS.
 - REPORT IMMEDIATELY TO THE FACILITY OWNER ANY BREAK, LEAK, DENT, GOUGE, OR OTHER DAMAGE TO THE FACILITY OWNER'S LINES MADE OR DISCOVERED DURING THE COURSE OF EXCAVATION OR DEMOLITION WORK, INCLUDING, DAMAGE TO PIPELINE COATINGS, LOCATING WIRE AND CABLES FOR CATHODIC PROTECTION.
 - IN THE EVENT OF AN EMERGENCY INVOLVING DANGER TO LIFE, HEALTH OR PROPERTY AS A RESULT OF DAMAGE TO AN UNDERGROUND FACILITY CONTAINING GAS PRODUCTS OR AS A RESULT OF AN ELECTRICAL SHORT OR ESCAPE OF GAS OR HAZARDOUS LIQUIDS, THE EXCAVATOR SHALL:
A. PROCEED TO EVACUATE HIS OR HER EMPLOYEES AND ALL OTHER ENDANGERED PERSONS FROM THE IMMEDIATE VICINITY TO THE BEST OF HIS OR HER ABILITY.
B. IMMEDIATELY CONTACT 911 AND THE FACILITY OWNER, REPORTING THE EXACT LOCATION, NATURE OF THE EMERGENCY AND TYPE OF UNDERGROUND FACILITY DAMAGED.
 - IN THE EVENT THAT A GAS CROSSING IS DEEMED NECESSARY, THE NATIONAL FUEL EMERGENCY NUMBER IS 1-800-444-3130.
 - ADJUSTING SERVICE BOXES, ETC. UNLESS OTHERWISE SPECIFIED, ALL TELEPHONE, ELECTRIC AND GAS VALVES, SERVICE BOXES AND COVERS SHALL BE ADJUSTED TO THE FINISHED SURFACES BY THE UTILITY COMPANIES THAT OWN THESE FACILITIES.
 - THERE IS UNDERGROUND TRAFFIC SIGNAL EQUIPMENT, VEHICLE DETECTORS, INTERCONNECTS, AND/OR INTELLIGENT TRANSPORTATION MONITORING SYSTEM WITHIN THE PERMIT WORK AREA. NYSDOT UNDERGROUND FACILITIES ARE NOT MAPPED OR MARKED AS PART OF THE "ONE CALL" 811 LOCATING SERVICE. FIVE (5) BUSINESS DAYS BEFORE ANY WORK IS STARTED, YOU MUST CONTACT THE DOT TRAFFIC SIGNAL SHOP (716) 649-1426 TO LOCATE THESE FACILITIES. THE PERMITTEE IS RESPONSIBLE FOR ALL COSTS TO RESTORE THIS EQUIPMENT IN ACCORDANCE WITH NYSDOT STANDARD & SPECIFICATIONS, WILL REIMBURSE THE STATE FOR INSPECTION SERVICES AND BE ASSESSED LIQUIDATED DAMAGES OF \$2,000 PER HOUR WHEN ANY SUCH SYSTEM IS OUT OF SERVICE.
 - UPON COMPLETION OF THE DRAINAGE STRUCTURE REPAIRS, CLEANING AND/OR REPLACEMENT A SURVEY SHOULD BE CONDUCTED TO ESTABLISH FINISH GRADE AND INLET/OUTLET ELEVATIONS OF ALL PIPES. THIS WORK IS INCLUDED IN ITEM 625.01.

| | | | | | |
|------------------------|--|--------------------------------------|--|-------------------|--|
| DRAWN BY DSJ | | ERIE COUNTY DEPT. OF PUBLIC WORKS | | SHEET NO. 2 OF 41 | |
| CHECKED BY GMS | | 95 FRANKLIN ST DIVISION OF HIGHWAYS | | DATE MAY 26 | |
| APPROVED BY DMS | | BUFFALO, N.Y. | | SCALE NONE | |
| DATE COMPLETED 5/13/26 | | 2026 CAPITAL OVERLAY B GENERAL NOTES | | NTS-1 | |

| TOTAL QUANTITIES | | | | |
|------------------|---|-------|--------------|------|
| ITEM NUMBER | DESCRIPTION | BOWEN | NORTH FOREST | UNIT |
| 203.24010017 | SHOULDER BACKUP MATERIAL | 1311 | 420 | TON |
| 206.03060005 | CONDUIT EXCAVATION AND BACKFILL (NOT IN ROADWAY) | 30 | 0 | LF |
| 404.0189 | TRUING AND LEVELING F9, ASPHALT, 80 SERIES COMPACTION | 0 | 40 | TON |
| 404.0973 | 9.5 F3 TOP COURSE ASPHALT, 70 SERIES COMPACTION | 5435 | 5725 | TON |
| 404.1979 | 19 F9 BINDER COURSE ASPHALT, 70 SERIES COMPACTION | 0 | 7633 | TON |
| 407.0102 | DILUTED TACK COAT | 3788 | 10228 | GAL |
| 418.7603 | ASPHALT PAVEMENT JOINT ADHESIVE | 17253 | 29024 | LF |
| 490.10 | PRODUCTION COLD MILLING OF BITUMINOUS CONCRETE | 0 | 60581 | SY |
| 490.30 | MISCELLANEOUS COLD MILLING OF BITUMINOUS CONCRETE | 2581 | 1259 | SY |
| 604.070500 | ALTERING DRAINAGE STRUCTURES, LEACHING BASINS AND MANHOLES (PARGE) | 0 | 8 | EA |
| 604.070501 | ALTERING DRAINAGE STRUCTURES, LEACHING BASINS AND MANHOLES (REPAIR) | 0 | 8 | EA |
| 604.10 | PREFABRICATED ADJUSTMENT RINGS FOR MANHOLES | 0 | 1 | EA |
| 608.020102 | ASPHALT SIDEWALKS, DRIVEWAYS AND BICYCLE PATHS, AND VEGETATION CONTROL STRIPS | 309 | 76 | TON |
| 619.01 | BASIC WORK ZONE TRAFFIC CONTROL | 1 | 1 | LS |
| 619.0901 | TEMPORARY PAVEMENT MARKINGS, STRIPES | 0 | 131338 | LF |
| 619.110512 | PORTABLE VARIABLE MESSAGE BOARDS | 2 | 2 | EA |
| 621.04 | CLEANING DRAINAGE STRUCTURES | 0 | 122 | EA |
| 625.01 | SURVEY OPERATIONS | 0 | 1 | LS |
| 680.53080305 | CONDUIT, FLEXIBLE, LIQUID TIGHT NON-METALLIC, 1" | 30 | 0 | LF |
| 680.54 | INDUCTANCE LOOP INSTALLATION | 517 | 0 | LF |
| 680.71 | SHIELDED LEAD IN CABLE | 30 | 0 | LF |
| 680.72 | INDUCTANCE LOOP WIRE | 1260 | 0 | LF |
| 685.1102 | WHITE EPOXY REFLECTORIZED PAVEMENT STRIPES - 20 MILS | 34506 | 21484 | LF |
| 685.1202 | YELLOW EPOXY REFLECTORIZED PAVEMENT STRIPES - 20 MILS | 28030 | 37047 | LF |
| 685.3104 | WHITE EPOXY REFLECTORIZED PAVEMENT SPECIAL STRIPES (HAND WORK) - 20 MILS | 0 | 4982 | LF |
| 685.3204 | YELLOW EPOXY REFLECTORIZED PAVEMENT SPECIAL STRIPES (HAND WORK) - 20 MILS | 0 | 1310 | EA |
| 685.3304 | WHITE EPOXY REFLECTORIZED PAVEMENT LETTERS - 20 MILS | 0 | 116 | EA |
| 685.3404 | WHITE EPOXY REFLECTORIZED PAVEMENT SYMBOLS - 20 MILS | 0 | 98 | EA |
| 697.03 | FIELD CHANGE PAYMENT | 1 | 1 | DC |
| 698.04 | ASPHALT PRICE ADJUSTMENT | 1 | 1 | DC |
| 698.05 | FUEL PRICE ADJUSTMENT | 1 | 1 | DC |
| 699.040001 | MOBILIZATION | 1 | 1 | LS |

| | | | | |
|----------------|---------|-----------------------------------|-----------|---------|
| DRAWN BY | DSJ | ERIE COUNTY DEPT. OF PUBLIC WORKS | SHEET NO. | 3 OF 41 |
| CHECKED BY | GMW | DIVISION OF HIGHWAYS | DATE | MAY 26 |
| APPROVED BY | DMS | 95 FRANKLIN ST BUFFALO, N.Y. | SCALE | NONE |
| DATE COMPLETED | 5/12/26 | 2026 CAPITAL OVERLAY B QUANTITIES | | QTY-1 |

| BOWEN RD DRIVEWAYS (WEST SIDE) | | | | | | |
|--------------------------------|---------------|-------------|------------|------------------|---------------------|-----------------------|
| HOUSE NUMBER | MATERIAL TYPE | LENGTH (FT) | WIDTH (FT) | ITEM 490.30 (SY) | ITEM 407.0102 (GAL) | ITEM 608.020102 (TON) |
| 25 | ASPHALT | 4.0 | 37.0 | 16.44 | 1.32 | 2.47 |
| 61 | ASPHALT | 4.0 | 17.0 | 7.56 | 0.60 | 1.13 |
| 73 | ASPHALT | 4.0 | 22.0 | 9.78 | 0.78 | 1.47 |
| 83 | ASPHALT | 4.0 | 25.0 | 11.11 | 0.89 | 1.67 |
| 95 | ASPHALT | 4.0 | 19.0 | 8.44 | 0.68 | 1.27 |
| 95 | ASPHALT | 4.0 | 22.0 | 9.78 | 0.78 | 1.47 |
| 127 | ASPHALT | 4.0 | 20.0 | 8.89 | 0.71 | 1.33 |
| 127 | ASPHALT | 4.0 | 19.0 | 8.44 | 0.68 | 1.27 |
| 141 | ASPHALT | 4.0 | 24.0 | 10.67 | 0.85 | 1.60 |
| 147 | ASPHALT | 4.0 | 59.0 | 26.22 | 2.10 | 3.93 |
| 155 | ASPHALT | 4.0 | 19.0 | 8.44 | 0.68 | 1.27 |
| 157 | ASPHALT | 4.0 | 30.0 | 13.33 | 1.07 | 2.00 |
| 175 | ASPHALT | 4.0 | 27.0 | 12.00 | 0.96 | 1.80 |
| 185 | ASPHALT | 4.0 | 23.0 | 10.22 | 0.82 | 1.53 |
| 203 | ASPHALT | 4.0 | 28.0 | 12.44 | 1.00 | 1.87 |
| 215 | ASPHALT | 4.0 | 26.0 | 11.56 | 0.92 | 1.73 |
| 221 | ASPHALT | 4.0 | 37.0 | 16.44 | 1.32 | 2.47 |
| 249 | STONE | 0.0 | 0.0 | 0.00 | 0.00 | 0.00 |
| 265 | ASPHALT | 4.0 | 37.0 | 16.44 | 1.32 | 2.47 |
| 275 | ASPHALT | 4.0 | 26.0 | 11.56 | 0.92 | 1.73 |
| 410 | ASPHALT | 4.0 | 23.0 | 10.22 | 0.82 | 1.53 |
| 440 | ASPHALT | 4.0 | 21.0 | 9.33 | 0.75 | 1.40 |
| 460 | STONE | 0.0 | 0.0 | 0.00 | 0.00 | 0.00 |
| 460 | STONE | 0.0 | 0.0 | 0.00 | 0.00 | 0.00 |
| 500 | ASPHALT | 4.0 | 26.0 | 11.56 | 0.92 | 1.73 |
| 510 | ASPHALT | 4.0 | 21.0 | 9.33 | 0.75 | 1.40 |
| 600 | ASPHALT | 4.0 | 44.0 | 19.56 | 1.56 | 2.93 |
| 610 | ASPHALT | 4.0 | 35.0 | 15.56 | 1.24 | 2.33 |
| 650 | ASPHALT | 4.0 | 29.0 | 12.89 | 1.03 | 1.93 |
| 660 | ASPHALT | 4.0 | 26.0 | 11.56 | 0.92 | 1.73 |
| 670 | ASPHALT | 4.0 | 61.0 | 27.11 | 2.17 | 4.07 |
| 680 | ASPHALT | 4.0 | 26.0 | 11.56 | 0.92 | 1.73 |
| 700 | ASPHALT | 4.0 | 29.0 | 12.89 | 1.03 | 1.93 |
| 710 | ASPHALT | 4.0 | 39.0 | 17.33 | 1.39 | 2.60 |
| 780 | ASPHALT | 4.0 | 25.0 | 11.11 | 0.89 | 1.67 |
| 842 | ASPHALT | 4.0 | 26.0 | 11.56 | 0.92 | 1.73 |
| 862 | ASPHALT | 4.0 | 33.0 | 14.67 | 1.17 | 2.20 |
| 880 | ASPHALT | 4.0 | 25.0 | 11.11 | 0.89 | 1.67 |
| 900 | ASPHALT | 4.0 | 30.0 | 13.33 | 1.07 | 2.00 |
| 920 | ASPHALT | 4.0 | 35.0 | 15.56 | 1.24 | 2.33 |
| 950 | ASPHALT | 4.0 | 27.0 | 12.00 | 0.96 | 1.80 |
| 1010 | ASPHALT | 4.0 | 37.0 | 16.44 | 1.32 | 2.47 |
| 1100 | ASPHALT | 4.0 | 27.0 | 12.00 | 0.96 | 1.80 |
| 1120 | ASPHALT | 4.0 | 27.0 | 12.00 | 0.96 | 1.80 |
| 1150 | ASPHALT | 4.0 | 17.0 | 7.56 | 0.60 | 1.13 |
| 1158 | ASPHALT | 4.0 | 21.0 | 9.33 | 0.75 | 1.40 |
| 1170 | ASPHALT | 4.0 | 24.0 | 10.67 | 0.85 | 1.60 |
| 1180 | ASPHALT | 4.0 | 21.0 | 9.33 | 0.75 | 1.40 |
| 1190 | ASPHALT | 4.0 | 21.0 | 9.33 | 0.75 | 1.40 |
| 1200 | ASPHALT | 4.0 | 26.0 | 11.56 | 0.92 | 1.73 |
| 1230 | ASPHALT | 4.0 | 53.0 | 23.56 | 1.88 | 3.53 |
| 1230 | ASPHALT | 4.0 | 56.0 | 24.89 | 1.99 | 3.73 |
| 1230 | ASPHALT | 4.0 | 48.0 | 21.33 | 1.71 | 3.20 |
| 661 | ASPHALT | 4.0 | 53.0 | 23.56 | 1.88 | 3.53 |
| 661 | ASPHALT | 4.0 | 56.0 | 24.89 | 1.99 | 3.73 |
| MOOG | ASPHALT | 4.0 | 33.0 | 14.67 | 1.17 | 2.20 |
| 1410 | ASPHALT | 4.0 | 27.0 | 12.00 | 0.96 | 1.80 |
| 1420 | ASPHALT | 4.0 | 34.0 | 15.11 | 1.21 | 2.27 |
| 1424 | ASPHALT | 4.0 | 21.0 | 9.33 | 0.75 | 1.40 |
| 1424 | ASPHALT | 4.0 | 32.0 | 14.22 | 1.14 | 2.13 |
| 1430 | ASPHALT | 4.0 | 25.0 | 11.11 | 0.89 | 1.67 |
| 1440 | ASPHALT | 4.0 | 27.0 | 12.00 | 0.96 | 1.80 |
| 1490 | ASPHALT | 4.0 | 27.0 | 12.00 | 0.96 | 1.80 |
| 1500 | ASPHALT | 4.0 | 27.0 | 12.00 | 0.96 | 1.80 |
| 1510 | ASPHALT | 4.0 | 31.0 | 13.78 | 1.10 | 2.07 |
| 1520 | ASPHALT | 4.0 | 25.0 | 11.11 | 0.89 | 1.67 |

| BOWEN RD DRIVEWAYS (WEST SIDE) | | | | | | |
|--------------------------------|---------------|-------------|------------|------------------|---------------------|-----------------------|
| HOUSE NUMBER | MATERIAL TYPE | LENGTH (FT) | WIDTH (FT) | ITEM 490.30 (SY) | ITEM 407.0102 (GAL) | ITEM 608.020102 (TON) |
| 1530 | STONE | 0.0 | 0.0 | 0.00 | 0.00 | 0.00 |
| 1540 | ASPHALT | 4.0 | 22.0 | 9.78 | 0.78 | 1.47 |
| 1550 | ASPHALT | 4.0 | 25.0 | 11.11 | 0.89 | 1.67 |
| ELMA TOWN HALL | ASPHALT | 4.0 | 102.0 | 45.33 | 3.63 | 6.80 |
| 1650 | ASPHALT | 4.0 | 25.0 | 11.11 | 0.89 | 1.67 |
| ELMA HIGHWAY DEPT. | ASPHALT | 4.0 | 141.0 | 62.67 | 5.01 | 9.40 |
| 1740 | ASPHALT | 4.0 | 30.0 | 13.33 | 1.07 | 2.00 |
| 1750 | ASPHALT | 4.0 | 35.0 | 15.56 | 1.24 | 2.33 |
| 1760 | STONE | 0.0 | 0.0 | 0.00 | 0.00 | 0.00 |
| 1790 | ASPHALT | 4.0 | 31.0 | 13.78 | 1.10 | 2.07 |
| 1800 | STONE | 0.0 | 0.0 | 0.00 | 0.00 | 0.00 |
| 1820 | ASPHALT | 4.0 | 29.0 | 12.89 | 1.03 | 1.93 |
| 1830 | ASPHALT | 4.0 | 22.0 | 9.78 | 0.78 | 1.47 |
| 1850 | ASPHALT | 4.0 | 26.0 | 11.56 | 0.92 | 1.73 |
| 1860 | ASPHALT | 4.0 | 43.0 | 19.11 | 1.53 | 2.87 |

| BOWEN RD DRIVEWAYS (EAST SIDE) | | | | | | |
|--------------------------------|---------------|-------------|------------|------------------|---------------------|-----------------------|
| HOUSE NUMBER | MATERIAL TYPE | LENGTH (FT) | WIDTH (FT) | ITEM 490.30 (SY) | ITEM 407.0102 (GAL) | ITEM 608.020102 (TON) |
| 841 | ASPHALT | 4.0 | 22.0 | 9.78 | 0.78 | 1.47 |
| 861 | ASPHALT | 4.0 | 20.0 | 8.89 | 0.71 | 1.33 |
| 871 | ASPHALT | 4.0 | 31.0 | 13.78 | 1.10 | 2.07 |
| 931 | STONE | 0.0 | 0.0 | 0.00 | 0.00 | 0.00 |
| 941 | ASPHALT | 4.0 | 31.0 | 13.78 | 1.10 | 2.07 |
| 951 | ASPHALT | 4.0 | 22.0 | 9.78 | 0.78 | 1.47 |
| 951 | ASPHALT | 4.0 | 35.0 | 15.56 | 1.24 | 2.33 |
| 981 | ASPHALT | 4.0 | 83.0 | 36.89 | 2.95 | 5.53 |
| 1051 | ASPHALT | 4.0 | 35.0 | 15.56 | 1.24 | 2.33 |
| 1061 | STONE | 0.0 | 0.0 | 0.00 | 0.00 | 0.00 |
| 1071 | ASPHALT | 4.0 | 18.0 | 8.00 | 0.64 | 1.20 |
| 1081 | ASPHALT | 4.0 | 24.0 | 10.67 | 0.85 | 1.60 |
| 1091 | ASPHALT | 4.0 | 25.0 | 11.11 | 0.89 | 1.67 |
| 1101 | STONE | 0.0 | 0.0 | 0.00 | 0.00 | 0.00 |
| 1111 | STONE | 0.0 | 0.0 | 0.00 | 0.00 | 0.00 |
| 1121 | STONE | 0.0 | 0.0 | 0.00 | 0.00 | 0.00 |
| 1141 | ASPHALT | 4.0 | 17.0 | 7.56 | 0.60 | 1.13 |
| 1151 | STONE | 0.0 | 0.0 | 0.00 | 0.00 | 0.00 |
| 1161 | ASPHALT | 4.0 | 23.0 | 10.22 | 0.82 | 1.53 |
| 1171 | ASPHALT | 4.0 | 19.0 | 8.44 | 0.68 | 1.27 |
| 1191 | ASPHALT | 4.0 | 51.0 | 22.67 | 1.81 | 3.40 |
| 1241 | ASPHALT | 4.0 | 45.0 | 20.00 | 1.60 | 3.00 |
| 1271 | ASPHALT | 4.0 | 94.0 | 41.78 | 3.34 | 6.27 |
| 1281 | STONE | 0.0 | 0.0 | 0.00 | 0.00 | 0.00 |
| 1291 | ASPHALT | 4.0 | 32.0 | 14.22 | 1.14 | 2.13 |
| 1301 | ASPHALT | 4.0 | 30.0 | 13.33 | 1.07 | 2.00 |
| 1301 | ASPHALT | 4.0 | 32.0 | 14.22 | 1.14 | 2.13 |
| 1331 | ASPHALT | 4.0 | 34.0 | 15.11 | 1.21 | 2.27 |
| 1341 | ASPHALT | 4.0 | 25.0 | 11.11 | 0.89 | 1.67 |
| 1351 | ASPHALT | 4.0 | 25.0 | 11.11 | 0.89 | 1.67 |
| 1371 | ASPHALT | 4.0 | 32.0 | 14.22 | 1.14 | 2.13 |
| 1391 | ASPHALT | 4.0 | 18.0 | 8.00 | 0.64 | 1.20 |
| 1401 | ASPHALT | 4.0 | 26.0 | 11.56 | 0.92 | 1.73 |
| 1411 | ASPHALT | 4.0 | 22.0 | 9.78 | 0.78 | 1.47 |
| 1441 | ASPHALT | 4.0 | 23.0 | 10.22 | 0.82 | 1.53 |
| 1441 | ASPHALT | 4.0 | 21.0 | 9.33 | 0.75 | 1.40 |
| 1475 | ASPHALT | 4.0 | 34.0 | 15.11 | 1.21 | 2.27 |
| 1485 | ASPHALT | 4.0 | 38.0 | 16.89 | 1.35 | 2.53 |
| 1485 | ASPHALT | 4.0 | 38.0 | 16.89 | 1.35 | 2.53 |
| 1485 | ASPHALT | 4.0 | 29.0 | 12.89 | 1.03 | 1.93 |
| 1491 | ASPHALT | 4.0 | 26.0 | 11.56 | 0.92 | 1.73 |
| 1507 | ASPHALT | 4.0 | 47.0 | 20.89 | 1.67 | 3.13 |
| 1511 | ASPHALT | 4.0 | 26.0 | 11.56 | 0.92 | 1.73 |
| 1531 | ASPHALT | 4.0 | 20.0 | 8.89 | 0.71 | 1.33 |
| 1541 | ASPHALT | 4.0 | 20.0 | 8.89 | 0.71 | 1.33 |
| 1561 | ASPHALT | 4.0 | 18.0 | 8.00 | 0.64 | 1.20 |
| 1561 | ASPHALT | 4.0 | 30.0 | 13.33 | 1.07 | 2.00 |
| 1581 | ASPHALT | 4.0 | 22.0 | 9.78 | 0.78 | 1.47 |
| 1605 | ASPHALT | 4.0 | 22.0 | 9.78 | 0.78 | 1.47 |
| 1615 | ASPHALT | 4.0 | 26.0 | 11.56 | 0.92 | 1.73 |
| 1641 | ASPHALT | 4.0 | 22.0 | 9.78 | 0.78 | 1.47 |
| 1651 | ASPHALT | 4.0 | 53.0 | 23.56 | 1.88 | 3.53 |
| 1671 | ASPHALT | 4.0 | 21.0 | 9.33 | 0.75 | 1.40 |
| 1675 | ASPHALT | 4.0 | 30.0 | 13.33 | 1.07 | 2.00 |
| 1681 | ASPHALT | 4.0 | 66.0 | 29.33 | 2.35 | 4.40 |
| 1731 | ASPHALT | 4.0 | 18.0 | 8.00 | 0.64 | 1.20 |
| 1731 | ASPHALT | 4.0 | 24.0 | 10.67 | 0.85 | 1.60 |
| 1751 | ASPHALT | 4.0 | 30.0 | 13.33 | 1.07 | 2.00 |
| 1761 | ASPHALT | 4.0 | 23.0 | 10.22 | 0.82 | 1.53 |
| 1771 | ASPHALT | 4.0 | 20.0 | 8.89 | 0.71 | 1.33 |
| 1771 | ASPHALT | 4.0 | 25.0 | 11.11 | 0.89 | 1.67 |
| 1801 | ASPHALT | 4.0 | 26.0 | 11.56 | 0.92 | 1.73 |
| 1811 | ASPHALT | 4.0 | 24.0 | 10.67 | 0.85 | 1.60 |
| 1821 | ASPHALT | 4.0 | 20.0 | 8.89 | 0.71 | 1.33 |
| 1841 | ASPHALT | 4.0 | 30.0 | 13.33 | 1.07 | 2.00 |

| BOWEN RD DRIVEWAYS (EAST SIDE) | | | | | | |
|--------------------------------|---------------|-------------|------------|------------------|---------------------|-----------------------|
| HOUSE NUMBER | MATERIAL TYPE | LENGTH (FT) | WIDTH (FT) | ITEM 490.30 (SY) | ITEM 407.0102 (GAL) | ITEM 608.020102 (TON) |
| APARTMENTS | ASPHALT | 4.0 | 43.0 | 19.11 | 1.53 | 2.87 |
| 140 | ASPHALT | 4.0 | 18.0 | 8.00 | 0.64 | 1.20 |
| 140 | ASPHALT | 4.0 | 27.0 | 12.00 | 0.96 | 1.80 |
| 150 | ASPHALT | 4.0 | 19.0 | 8.44 | 0.68 | 1.27 |
| 160 | ASPHALT | 4.0 | 24.0 | 10.67 | 0.85 | 1.60 |
| 170 | ASPHALT | 4.0 | 28.0 | 12.44 | 1.00 | 1.87 |
| 511 | ASPHALT | 4.0 | 25.0 | 11.11 | 0.89 | 1.67 |
| 623 | ASPHALT | 4.0 | 37.0 | 16.44 | 1.32 | 2.47 |
| 771 | ASPHALT | 4.0 | 26.0 | 11.56 | 0.92 | 1.73 |
| 801 | ASPHALT | 4.0 | 29.0 | 12.89 | 1.03 | 1.93 |
| 821 | ASPHALT | 4.0 | 37.0 | 16.44 | 1.32 | 2.47 |

| NORTH FOREST RD DRIVEWAYS (WEST SIDE) | | | | | | |
|---------------------------------------|---------------|-------------|------------|------------------|---------------------|-----------------------|
| HOUSE NUMBER | MATERIAL TYPE | LENGTH (FT) | WIDTH (FT) | ITEM 490.30 (SY) | ITEM 407.0102 (GAL) | ITEM 608.020102 (TON) |
| 2360 | ASPHALT | 4.0 | 30.00 | 13.33 | 1.07 | 1.60 |
| 2390 | ASPHALT | 4.0 | 57.00 | 25.33 | 2.03 | 3.04 |
| 2410 | ASPHALT | 4.0 | 89.00 | 39.56 | 3.16 | 4.75 |
| 2430 | ASPHALT | 4.0 | 48.00 | 21.33 | 1.71 | 2.56 |
| 2450 | ASPHALT | 4.0 | 21.00 | 9.33 | 0.75 | 1.12 |
| 2450 | ASPHALT | 4.0 | 18.00 | 8.00 | 0.64 | 0.96 |
| 2480 | ASPHALT | 4.0 | 30.00 | 13.33 | 1.07 | 1.60 |
| 2480 | ASPHALT | 4.0 | 24.00 | 10.67 | 0.85 | 1.28 |
| 2514 | ASPHALT | 4.0 | 22.00 | 9.78 | 0.78 | 1.17 |
| 2514 | ASPHALT | 4.0 | 24.00 | 10.67 | 0.85 | 1.28 |
| 2546 | ASPHALT | 4.0 | 24.00 | 10.67 | 0.85 | 1.28 |
| 2554 | ASPHALT | 4.0 | 24.00 | 10.67 | 0.85 | 1.28 |
| 2560 | ASPHALT | 4.0 | 22.00 | 9.78 | 0.78 | 1.17 |
| 2610 | ASPHALT | 4.0 | 24.00 | 10.67 | 0.85 | 1.28 |
| 2640 | ASPHALT | 4.0 | 58.00 | 25.78 | 2.06 | 3.09 |
| 2640 | ASPHALT | 4.0 | 65.00 | 28.89 | | |

| NORTH FOREST ROAD | | | |
|--------------------------|-----------------------|----------------------------|-------------------------|
| EXISTING DRAINAGE NUMBER | ITEM 604.070500 PARGE | ITEM 604.070501 DS REPAIRS | ITEM 621.04 CLEANING DS |
| DI-1 | | | 1 |
| DI-2 | 1 | 1 | |
| DI-3 | 1 | 1 | |
| DI-4 | 1 | 1 | |
| DI-5 | | | 1 |
| DI-6 | | | 1 |
| DI-7 | | | 1 |
| DI-8 | | | 1 |
| DI-9 | | | 1 |
| DI-10 | | | 1 |
| DI-11 | | | 1 |
| DI-12 | | | 1 |
| DI-13 | | | 1 |
| DI-14 | | | 1 |
| DI-15 | | | 1 |
| DI-16 | | | 1 |
| DI-17 | | | 1 |
| DI-18 | | | 1 |
| DI-19 | | | 1 |
| DI-20 | | | 1 |
| DI-21 | | | 1 |
| DI-22 | | | 1 |
| DI-23 | | | 1 |
| DI-24 | | | 1 |
| DI-25 | | | 1 |
| DI-26 | | | 1 |
| DI-27 | | | 1 |
| DI-28 | | | 1 |
| DI-29 | | | 1 |
| DI-30 | | | 1 |
| DI-31 | | | 1 |
| DI-32 | | | 1 |
| DI-33 | | | 1 |
| DI-34 | 1 | 1 | |
| DI-35 | | | 1 |
| DI-36 | | | 1 |
| DI-37 | | | 1 |
| DI-38 | | | 1 |
| DI-39 | | | 1 |
| DI-40 | | | 1 |
| DI-41 | | | 1 |
| DI-42 | | | 1 |
| DI-43 | | | 1 |
| DI-44 | | | 1 |
| DI-45 | | | 1 |
| DI-46 | | | 1 |
| DI-47 | | | 1 |
| DI-48 | | | 1 |
| DI-49 | | | 1 |
| DI-50 | | | 1 |
| DI-51 | | | 1 |
| DI-52 | | | 1 |
| DI-53 | | | 1 |
| DI-54 | | | 1 |
| DI-55 | | | 1 |
| DI-56 | | | 1 |
| DI-57 | | | 1 |
| DI-58 | | | 1 |
| DI-59 | | | 1 |
| DI-60 | | | 1 |
| DI-61 | | | 1 |
| DI-62 | | | 1 |
| DI-63 | | | 1 |
| DI-64 | | | 1 |
| DI-65 | | | 1 |

| NORTH FOREST ROAD | | | |
|--------------------------|-----------------------|----------------------------|-------------------------|
| EXISTING DRAINAGE NUMBER | ITEM 604.070500 PARGE | ITEM 604.070501 DS REPAIRS | ITEM 621.04 CLEANING DS |
| DI-66 | | | 1 |
| DI-67 | | | 1 |
| DI-68 | | | 1 |
| DI-69 | 1 | 1 | |
| DI-70 | | | 1 |
| DI-71 | | | 1 |
| DI-72 | | | 1 |
| DI-73 | | | 1 |
| DI-74 | 1 | 1 | |
| DI-75 | 1 | 1 | |
| DI-76 | | | 1 |
| DI-77 | | | 1 |
| DI-78 | | | 1 |
| DI-79 | | | 1 |
| DI-80 | | | 1 |
| DI-81 | | | 1 |
| DI-82 | | | 1 |
| DI-83 | | | 1 |
| DI-84 | | | 1 |
| DI-85 | | | 1 |
| DI-86 | | | 1 |
| DI-87 | | | 1 |
| DI-88 | | | 1 |
| DI-89 | | | 1 |
| DI-90 | | | 1 |
| DI-91 | | | 1 |
| DI-92 | | | 1 |
| DI-93 | | | 1 |
| DI-94 | | | 1 |
| DI-95 | | | 1 |
| DI-96 | | | 1 |
| DI-97 | | | 1 |
| DI-98 | | | 1 |
| DI-99 | | | 1 |
| DI-100 | | | 1 |
| DI-101 | | | 1 |
| DI-102 | | | 1 |
| DI-103 | | | 1 |
| DI-104 | | | 1 |
| DI-105 | | | 1 |
| DI-106 | | | 1 |
| DI-107 | | | 1 |
| DI-108 | | | 1 |
| DI-109 | | | 1 |
| DI-110 | | | 1 |
| DI-111 | | | 1 |
| DI-112 | | | 1 |
| DI-113 | | | 1 |
| DI-114 | | | 1 |
| DI-115 | | | 1 |
| DI-116 | | | 1 |
| DI-117 | | | 1 |
| DI-118 | | | 1 |
| DI-119 | | | 1 |
| DI-120 | | | 1 |
| DI-121 | | | 1 |
| DI-122 | | | 1 |
| DI-123 | | | 1 |
| DI-124 | | | 1 |
| DI-125 | | | 1 |
| DI-126 | | | 1 |
| DI-127 | | | 1 |
| DI-128 | 1 | 1 | |
| DI-129 | | | 1 |
| DI-130 | | | 1 |

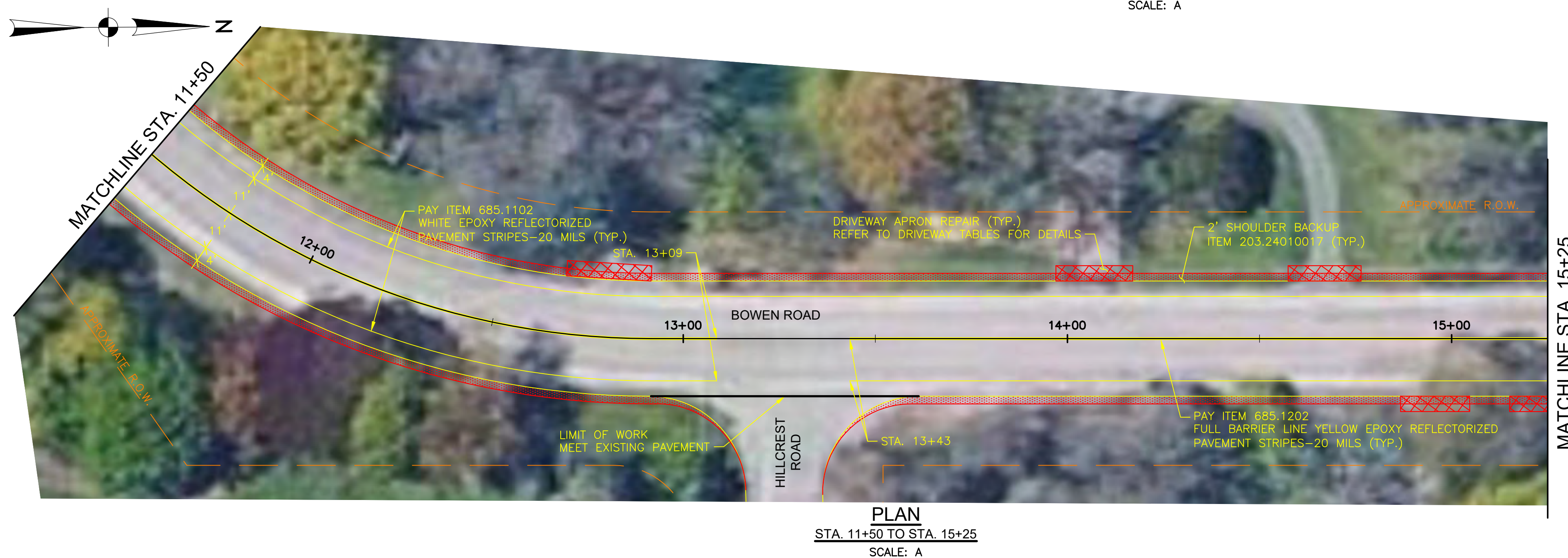
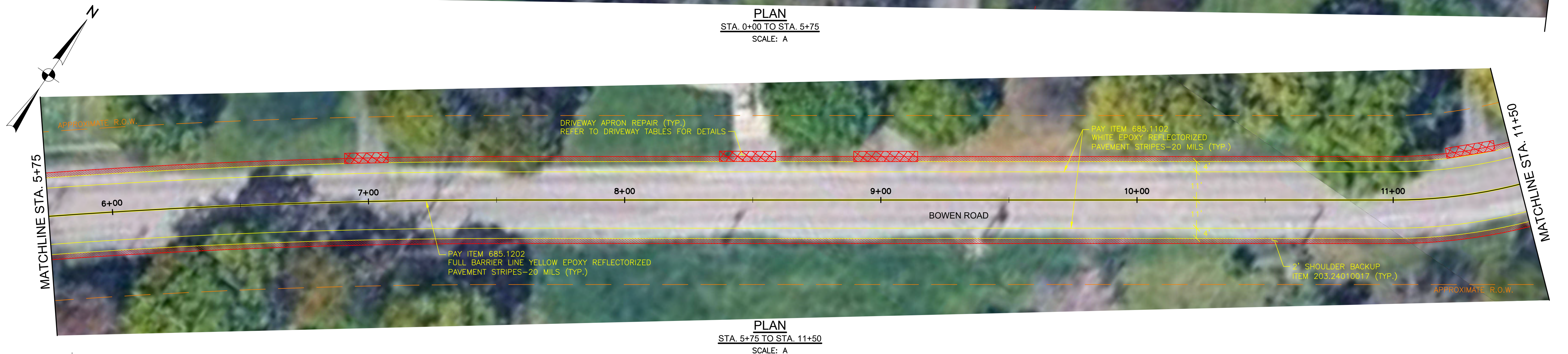
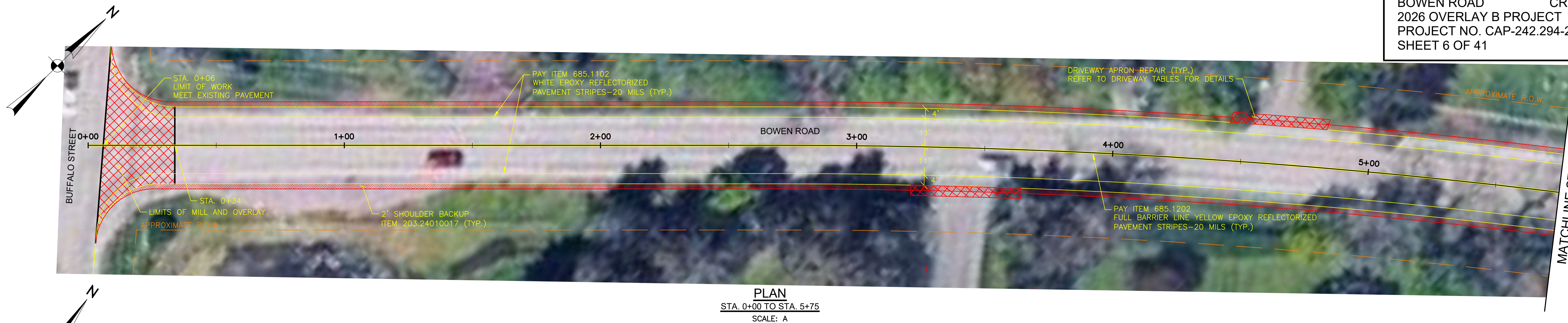
| DETECTOR DATA TABLE - BOWEN AND RICE | | | | | | | | | | | |
|--------------------------------------|--------------|---------------|------------------|-------|---------------|----------|-------------|-------------|-------------|-------------------|-------------------|
| LOCATION | TRAFFIC TYPE | DETECTOR TYPE | INDUCTANCE LOOPS | | | MODE | ITEM 680.54 | ITEM 680.72 | ITEM 680.71 | ITEM 206.03060005 | ITEM 680.53080305 |
| | | | SHAPE | TURNS | SIZE | | | | | | |
| NORTHBOUND | VEH. | LOOP | QUADRUPOLE | 2-4-2 | 20' x 6' (EA) | PRESENCE | 92 | 214 | 10 | 10 | 10 |
| | VEH. | LOOP | RECTANGULAR | 3 | 20' x 6' (EA) | PRESENCE | 72 | 186 | | | |

- NOTES: 1. ALL DETECTORS SHALL BE INSTALLED IN CONFORMANCE WITH NYS DOT STANDARD SHEETS 680-14.
2. THE LEADING EDGE OF OF THE FIRST LOOP SHALL BE INSTALLED 1 FT. IN FRONT OF THE STOP BAR.
3. IN EACH TRAVEL LANE, THE FRONT LOOP DETECTORS SHALL BE 2-4-2 QUADRUPOLE LOOPS FOLLOWED BY 10 FOOT GAP, THEN RECTANGULAR LOOP.
4. THE PRESENCE OF ALL EXISTING LOOPS MUST BE FIELD VERIFIED BY THE CONTRACTOR.

| DETECTOR DATA TABLE - BOWEN AND JANISON | | | | | | | | | | | |
|---|--------------|---------------|------------------|-------|---------------|----------|-------------|-------------|-------------|-------------------|-------------------|
| LOCATION | TRAFFIC TYPE | DETECTOR TYPE | INDUCTANCE LOOPS | | | MODE | ITEM 680.54 | ITEM 680.72 | ITEM 680.71 | ITEM 206.03060005 | ITEM 680.53080305 |
| | | | SHAPE | TURNS | SIZE | | | | | | |
| NORTHBOUND | VEH. | LOOP | QUADRUPOLE | 2-4-2 | 20' x 6' (EA) | PRESENCE | 92 | 214 | 10 | 10 | 10 |
| | VEH. | LOOP | RECTANGULAR | 3 | 20' x 6' (EA) | PRESENCE | 72 | 186 | | | |
| SOUTHBOUND | VEH. | LOOP | QUADRUPOLE | 2-4-2 | 20' x 6' (EA) | PRESENCE | 92 | 214 | 10 | 10 | 10 |
| | VEH. | LOOP | RECTANGULAR | 3 | 20' x 6' (EA) | PRESENCE | 72 | 186 | | | |

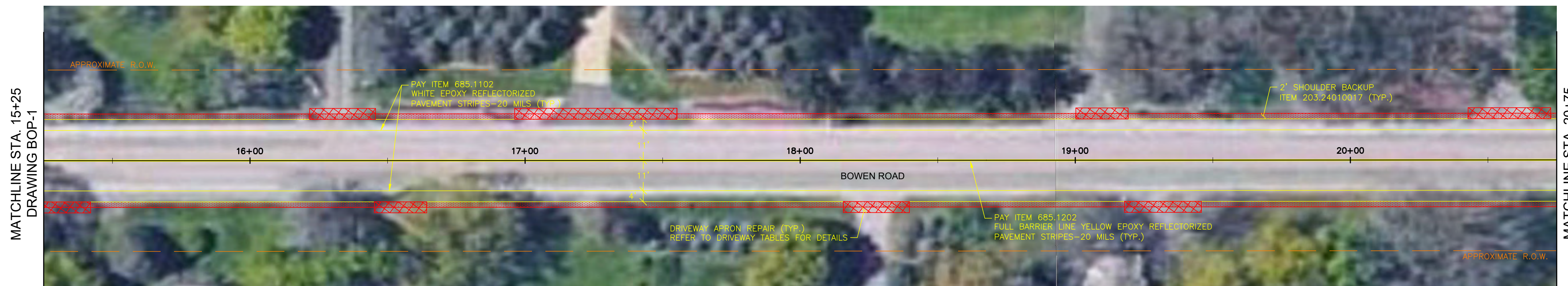
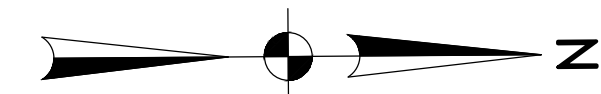
- NOTES: 1. ALL DETECTORS SHALL BE INSTALLED IN CONFORMANCE WITH NYS DOT STANDARD SHEETS 680-14.
2. THE LEADING EDGE OF OF THE FIRST LOOP SHALL BE INSTALLED 1 FT. IN FRONT OF THE STOP BAR.
3. IN EACH TRAVEL LANE, THE FRONT LOOP DETECTORS SHALL BE 2-4-2 QUADRUPOLE LOOPS FOLLOWED BY 10 FOOT GAP, THEN RECTANGULAR LOOP.
4. THE PRESENCE OF ALL EXISTING LOOPS MUST BE FIELD VERIFIED BY THE CONTRACTOR.

| | | |
|------------------------|---|-------------------|
| DRAWN BY DSJ | ERIE COUNTY DEPT. OF PUBLIC WORKS DIVISION OF HIGHWAYS 95 FRANKLIN ST BUFFALO, N.Y. | SHEET NO. 5 OF 40 |
| CHECKED BY GMW | | DATE MAY 26 |
| APPROVED BY DMS | 2026 CAPITAL OVERLAY B TABLES (2 OF 2) | SCALE NONE |
| DATE COMPLETED 5/12/26 | | TAB-2 |

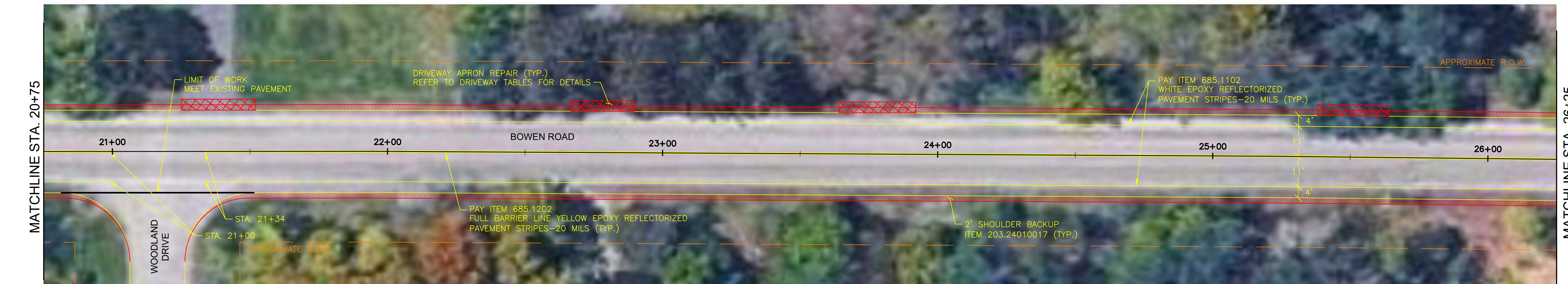
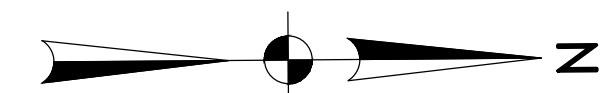


| | | |
|-------------------------|---|-------------------|
| DRAWN BY: DSJ | ERIE COUNTY DEPT. OF PUBLIC WORKS | SHEET NO. 6 OF 41 |
| CHECKED BY: GMW | DIVISION OF HIGHWAYS | DATE: MAY 26 |
| APPROVED BY: DMS | 95 FRANKLIN ST BUFFALO, N.Y. | SCALE: 1" = 20' |
| DATE COMPLETED: 5/12/26 | BOWEN ROAD - CR 242 MILL AND OVERLAY PLANS (1 OF 11) | BOP-1 |

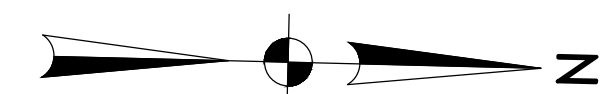
V:\Public Works\Highways - Main Road Files\Overlays\2026 Overlay Program\Bowen



PLAN
 STA. 15+25 TO STA. 20+75
 SCALE: A



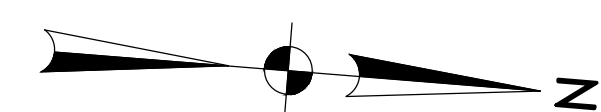
PLAN
 STA. 20+75 TO STA. 26+25
 SCALE: A



PLAN
 STA. 26+25 TO STA. 31+25
 SCALE: A



| | | |
|-------------------------|---|-------------------|
| DRAWN BY: DSJ | ERIE COUNTY DEPT. OF PUBLIC WORKS | SHEET NO. 7 OF 41 |
| CHECKED BY: GMW | DIVISION OF HIGHWAYS | DATE: MAY 26 |
| APPROVED BY: DMS | 95 FRANKLIN ST BUFFALO, N.Y. | SCALE: 1" = 20' |
| DATE COMPLETED: 5/12/26 | BOWEN ROAD - CR 242 MILL AND OVERLAY PLANS (2 OF 11) | BOP-2 |

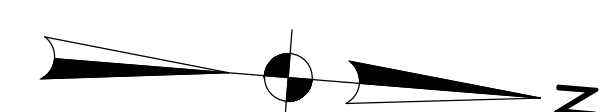


MATCHLINE STA. 31+25
 DRAWING BOP-2

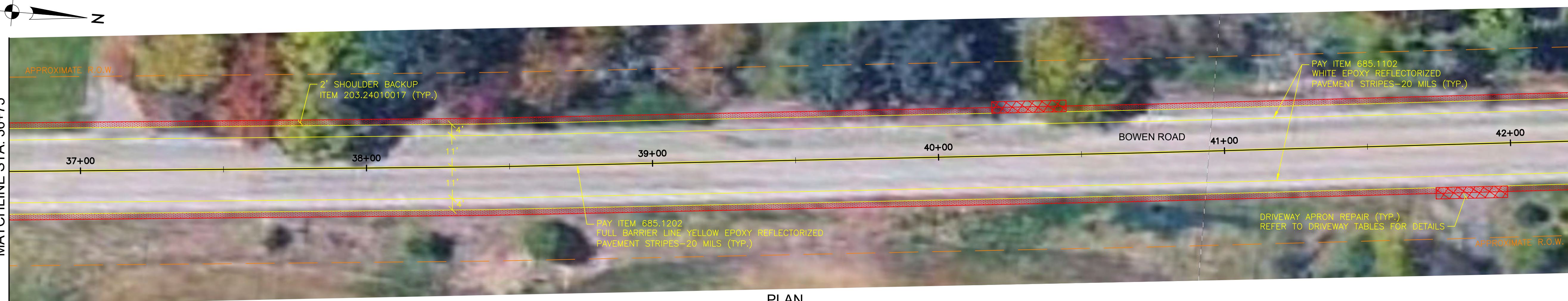


MATCHLINE STA. 36+75

PLAN
 STA. 31+25 TO STA. 36+75
 SCALE: A

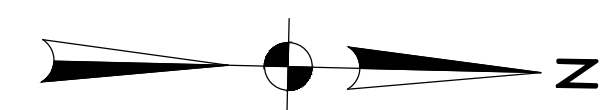


MATCHLINE STA. 36+75

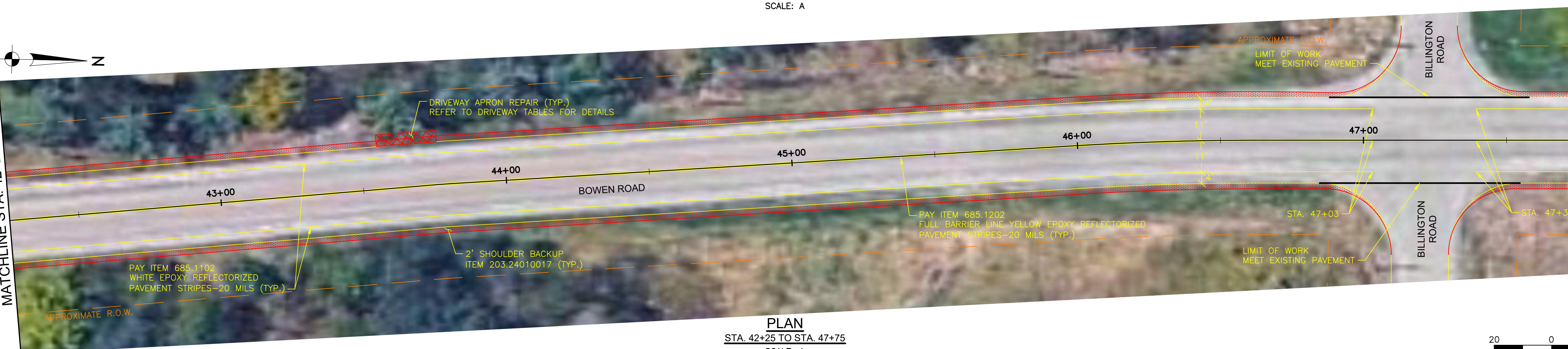


MATCHLINE STA. 42+25

PLAN
 STA. 36+75 TO STA. 42+25
 SCALE: A



MATCHLINE STA. 42+25



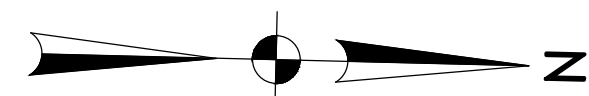
MATCHLINE STA. 47+75
 DRAWING BOP-4

PLAN
 STA. 42+25 TO STA. 47+75
 SCALE: A

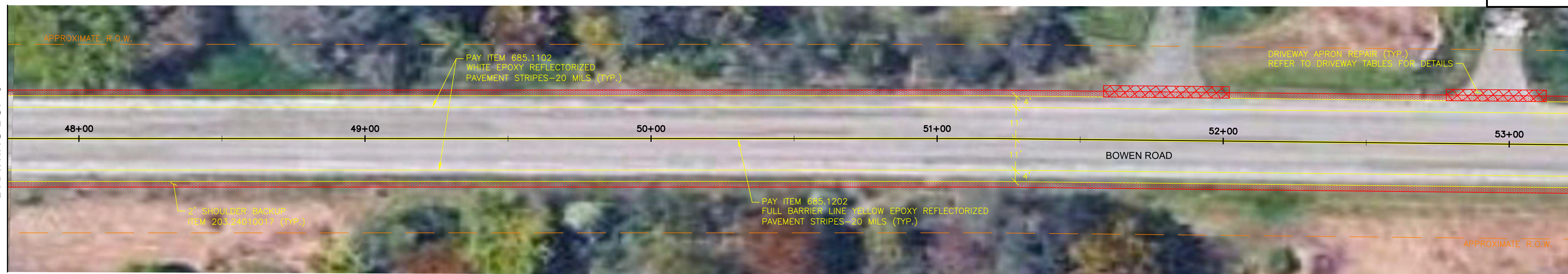


V:\Public Works\Highways - Main\Road Files\Overlays\2026 Overlay Program\Bowen

| | | |
|-------------------------|---|-------------------|
| DRAWN BY: DSJ | ERIE COUNTY DEPT. OF PUBLIC WORKS | SHEET NO. 8 OF 41 |
| CHECKED BY: GMW | DIVISION OF HIGHWAYS | DATE: MAY 26 |
| APPROVED BY: DMS | 95 FRANKLIN ST. BUFFALO, N.Y. | SCALE: 1" = 20' |
| DATE COMPLETED: 5/12/26 | BOWEN ROAD - CR 242 MILL AND OVERLAY PLANS (3 OF 11) | BOP-3 |



MATCHLINE STA. 47+75
 DRAWING BOP-3



MATCHLINE STA. 53+25

PLAN
 STA. 47+75 TO STA. 53+25
 SCALE: A

MATCHLINE STA. 53+25



MATCHLINE STA. 58+75

PLAN
 STA. 53+25 TO STA. 58+75
 SCALE: A

MATCHLINE STA. 58+75

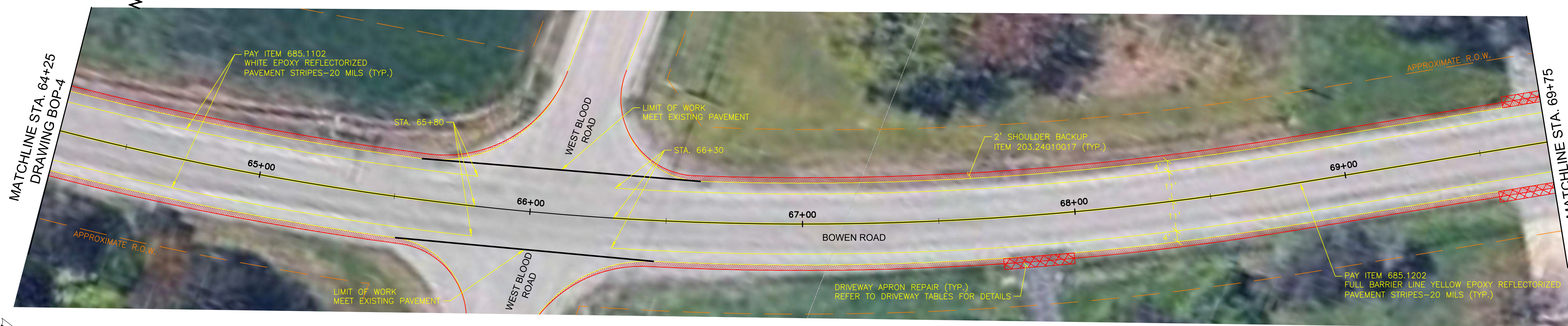


MATCHLINE STA. 64+25
 DRAWING BOP-5

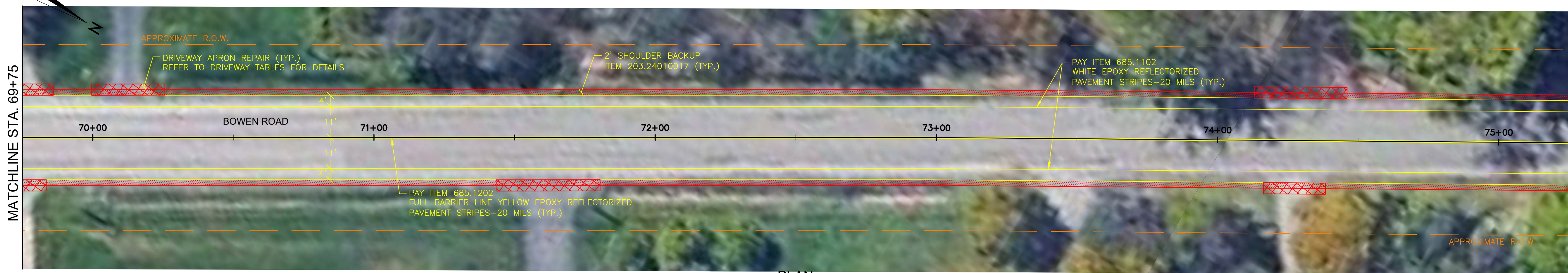
PLAN
 STA. 58+75 TO STA. 64+25
 SCALE: A



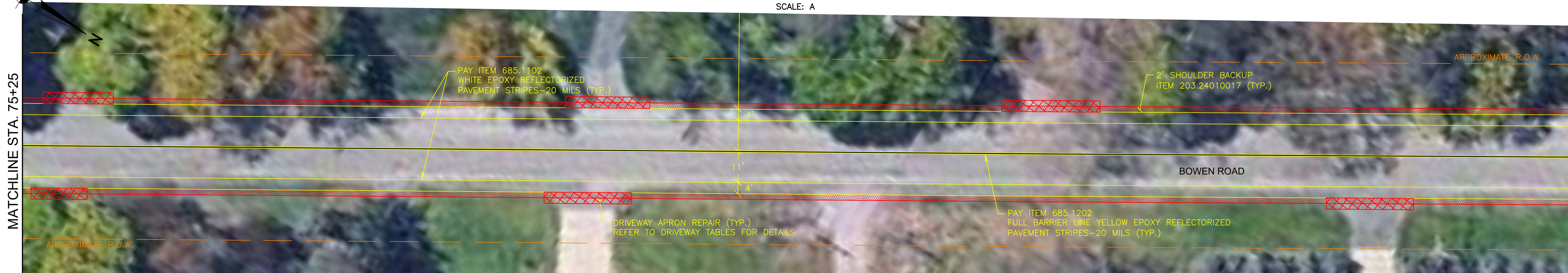
| | | |
|-------------------------|---|-------------------|
| DRAWN BY: DSJ | ERIE COUNTY DEPT. OF PUBLIC WORKS DIVISION OF HIGHWAYS | SHEET NO. 9 OF 41 |
| CHECKED BY: GMW | 95 FRANKLIN ST BUFFALO, N.Y. | DATE: MAY 26 |
| APPROVED BY: DMS | BOWEN ROAD - CR 242 MILL AND OVERLAY PLANS (4 OF 11) | SCALE: 1" = 20' |
| DATE COMPLETED: 5/12/26 | | BOP-4 |



PLAN
 STA. 64+25 TO STA. 69+75
 SCALE: A



PLAN
 STA. 69+75 TO STA. 75+25
 SCALE: A



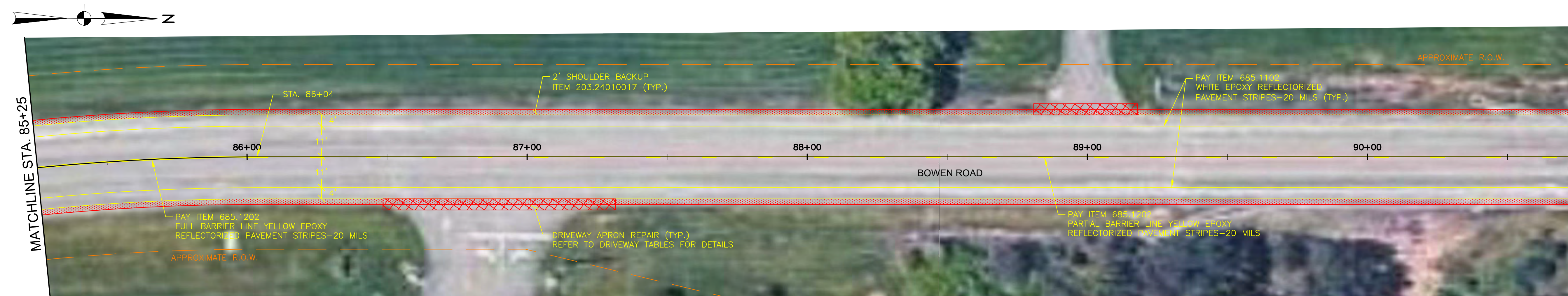
PLAN
 STA. 75+25 TO STA. 80+75
 SCALE: A



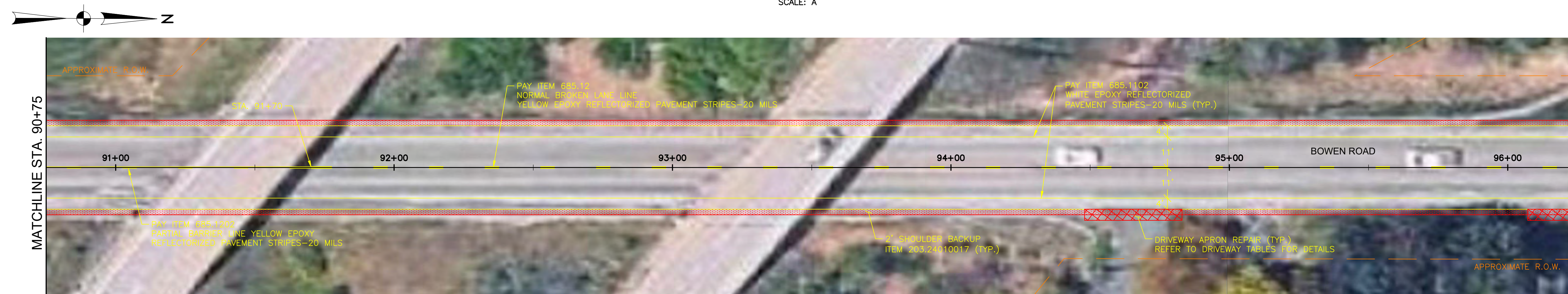
| | | |
|-------------------------|---|--------------------|
| DRAWN BY: DSJ | ERIE COUNTY DEPT. OF PUBLIC WORKS | SHEET NO. 10 OF 41 |
| CHECKED BY: GMW | DIVISION OF HIGHWAYS | DATE: MAY 26 |
| APPROVED BY: DMS | 95 FRANKLIN ST. BUFFALO, N.Y. | SCALE: 1" = 20' |
| DATE COMPLETED: 5/12/26 | BOWEN ROAD - CR 242 MILL AND OVERLAY PLANS (5 OF 11) | BOP-5 |



PLAN
 STA. 80+75 TO STA. 85+25
 SCALE: A



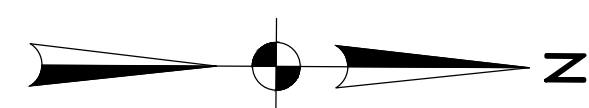
PLAN
 STA. 85+25 TO STA. 90+75
 SCALE: A



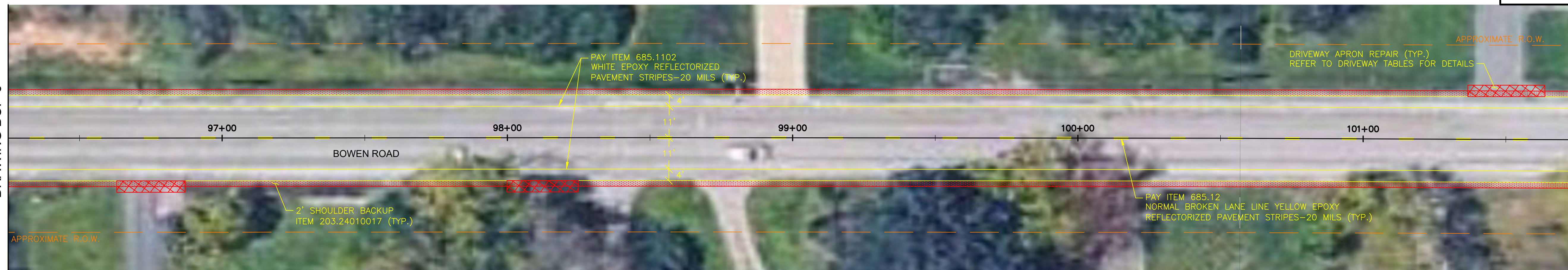
PLAN
 STA. 90+75 TO STA. 96+25
 SCALE: A



| | | |
|-------------------------|---|--------------------|
| DRAWN BY: DSJ | ERIE COUNTY DEPT. OF PUBLIC WORKS | SHEET NO. 11 OF 41 |
| CHECKED BY: GMW | DIVISION OF HIGHWAYS | DATE: MAY 26 |
| APPROVED BY: DMS | 95 FRANKLIN ST BUFFALO, N.Y. | SCALE: 1" = 20' |
| DATE COMPLETED: 5/12/26 | BOWEN ROAD - CR 242 MILL AND OVERLAY PLANS (6 OF 11) | BOP-6 |



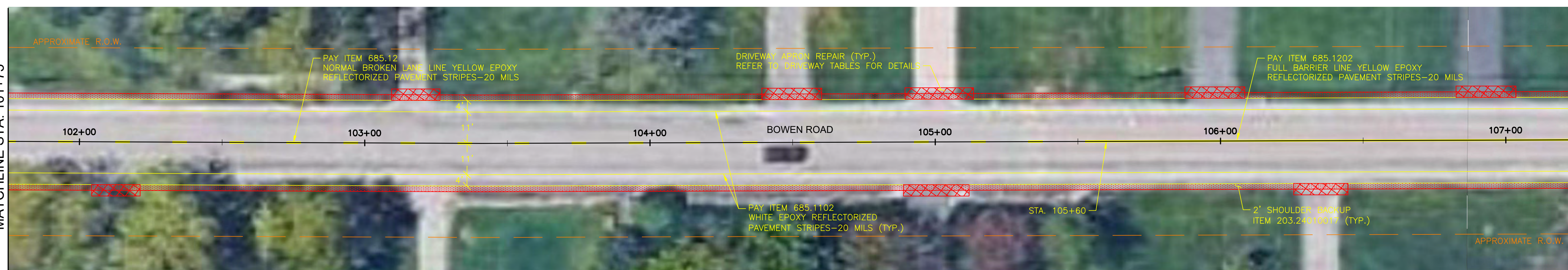
MATCHLINE STA. 96+25
DRAWING BOP-6



MATCHLINE STA. 101+75

PLAN
 STA. 96+25 TO STA. 101+75
 SCALE: A

MATCHLINE STA. 101+75



MATCHLINE STA. 107+25

PLAN
 STA. 101+75 TO STA. 107+25
 SCALE: A

MATCHLINE STA. 107+25
DRAWING BOP-8

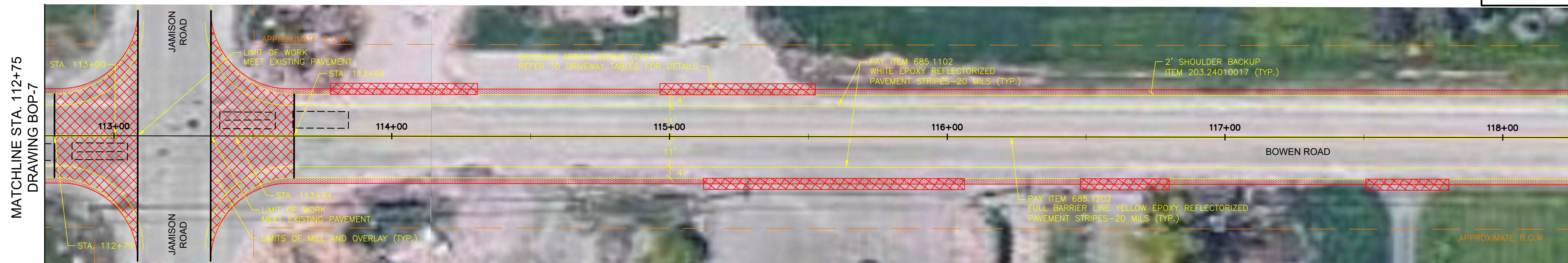
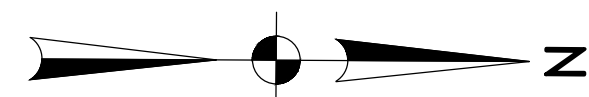


MATCHLINE STA. 112+75
DRAWING BOP-8

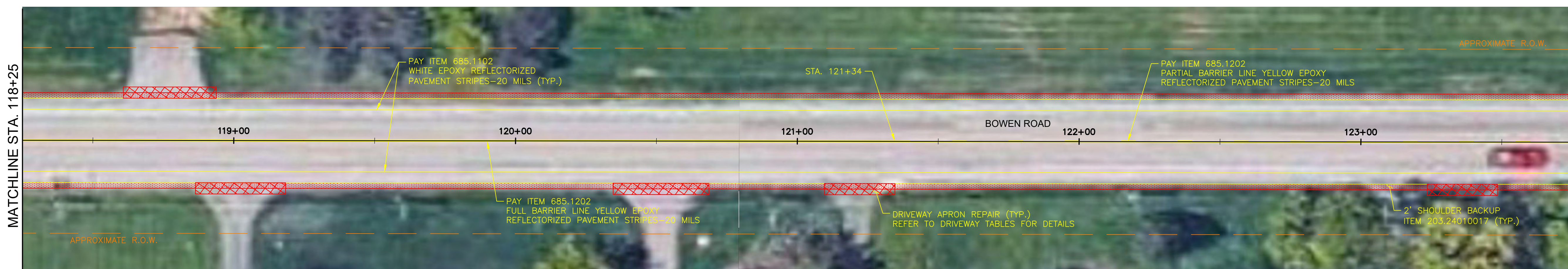
PLAN
 STA. 107+25 TO STA. 112+75
 SCALE: A



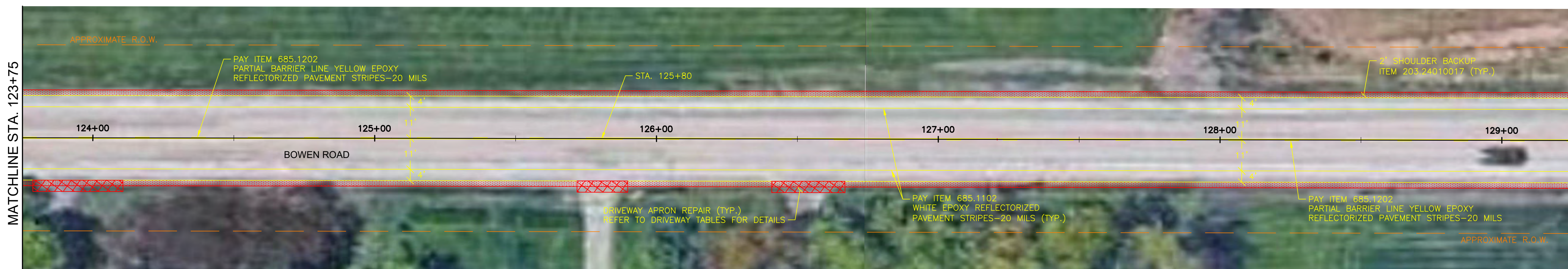
| | | |
|-------------------------|---|--------------------|
| DRAWN BY: DSJ | ERIE COUNTY DEPT. OF PUBLIC WORKS DIVISION OF HIGHWAYS | SHEET NO. 12 OF 41 |
| CHECKED BY: GMW | 95 FRANKLIN ST BUFFALO, N.Y. | DATE: MAY 26 |
| APPROVED BY: DMS | BOWEN ROAD - CR 242 MILL AND OVERLAY PLANS (7 OF 11) | SCALE: 1" = 20' |
| DATE COMPLETED: 5/12/26 | | BOP-7 |



PLAN
 STA. 112+75 TO STA. 118+25
 SCALE: A



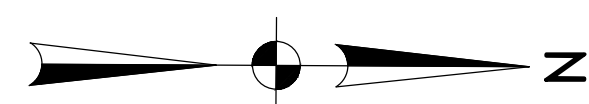
PLAN
 STA. 118+25 TO STA. 123+75
 SCALE: A



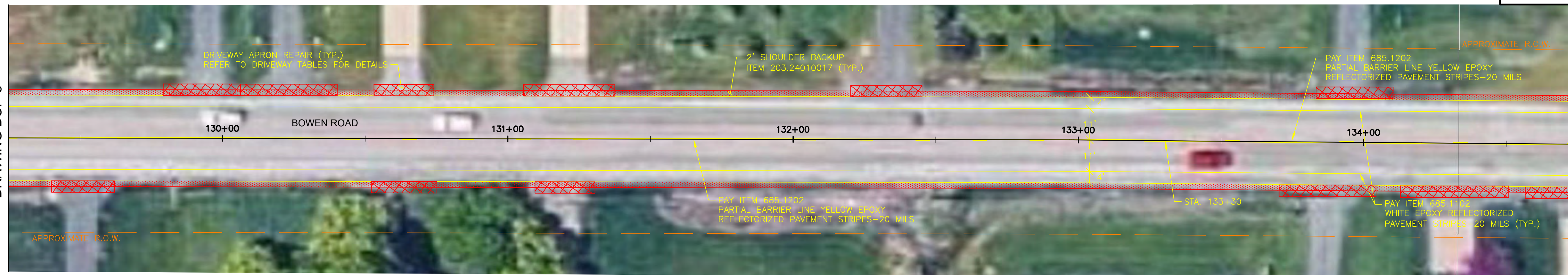
PLAN
 STA. 123+75 TO STA. 129+25
 SCALE: A



| | | |
|-------------------------|-----------------------------------|--------------------|
| DRAWN BY: DSJ | ERIE COUNTY DEPT. OF PUBLIC WORKS | SHEET NO. 13 OF 41 |
| CHECKED BY: GMW | DIVISION OF HIGHWAYS | DATE: MAY 26 |
| APPROVED BY: DMS | 95 FRANKLIN ST. BUFFALO, N.Y. | SCALE: 1" = 20' |
| DATE COMPLETED: 5/12/26 | BOWEN ROAD - CR 242 | BOP-8 |
| | MILL AND OVERLAY PLANS (8 OF 11) | |



MATCHLINE STA. 129+25
 DRAWING BOP-8



MATCHLINE STA. 134+75

PLAN
 STA. 129+25 TO STA. 134+75
 SCALE: A

MATCHLINE STA. 134+75



MATCHLINE STA. 140+25

PLAN
 STA. 134+75 TO STA. 140+25
 SCALE: A

MATCHLINE STA. 140+25

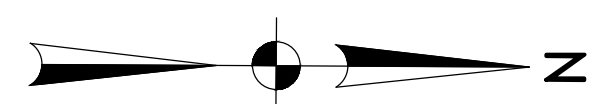


MATCHLINE STA. 145+75
 DRAWING BOP-10

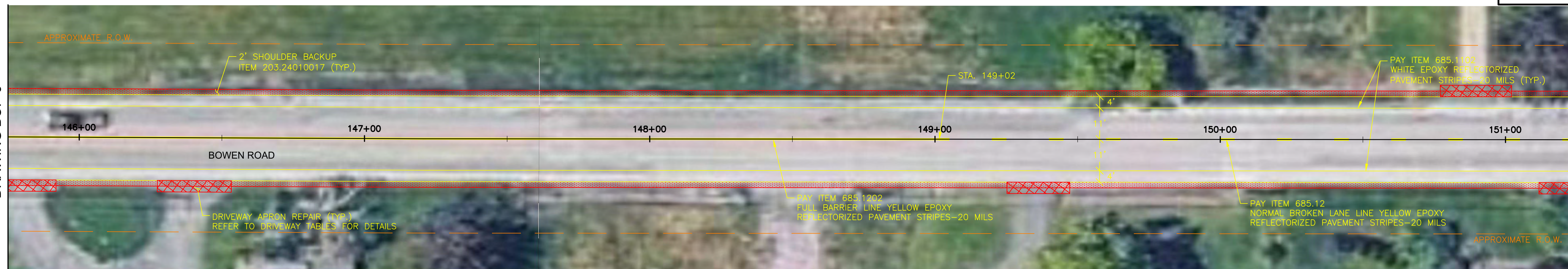
PLAN
 STA. 140+25 TO STA. 145+75
 SCALE: A



| | | |
|-------------------------|---|--------------------|
| DRAWN BY: DSJ | ERIE COUNTY DEPT. OF PUBLIC WORKS DIVISION OF HIGHWAYS | SHEET NO. 14 OF 41 |
| CHECKED BY: GMW | 95 FRANKLIN ST BUFFALO, N.Y. | DATE: MAY 26 |
| APPROVED BY: DMS | BOWEN ROAD - CR 242 MILL AND OVERLAY PLANS (9 OF 11) | SCALE: 1" = 20' |
| DATE COMPLETED: 5/12/26 | | BOP-9 |



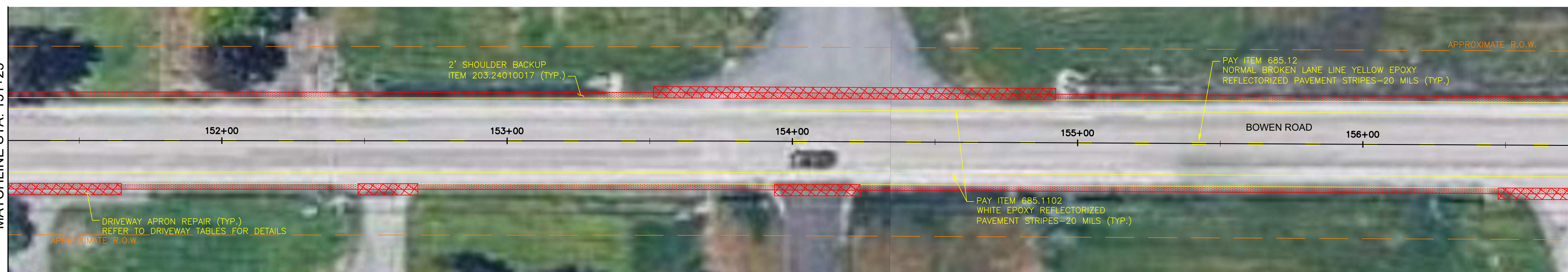
MATCHLINE STA. 145+75
 DRAWING BOP-9



MATCHLINE STA. 151+25

PLAN
 STA. 145+75 TO STA. 151+25
 SCALE: A

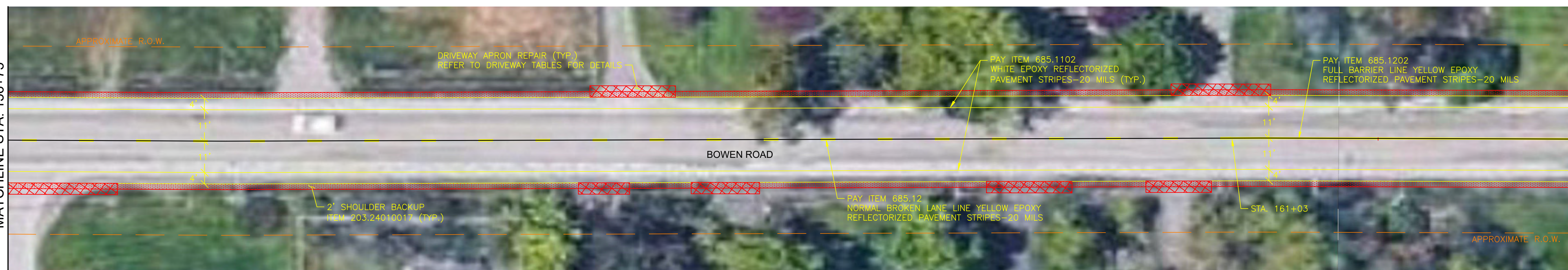
MATCHLINE STA. 151+25



MATCHLINE STA. 156+75

PLAN
 STA. 151+25 TO STA. 156+75
 SCALE: A

MATCHLINE STA. 156+75



MATCHLINE STA. 162+25
 DRAWING BOP-11

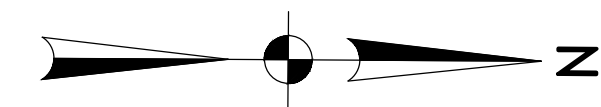
PLAN
 STA. 156+75 TO STA. 162+25
 SCALE: A



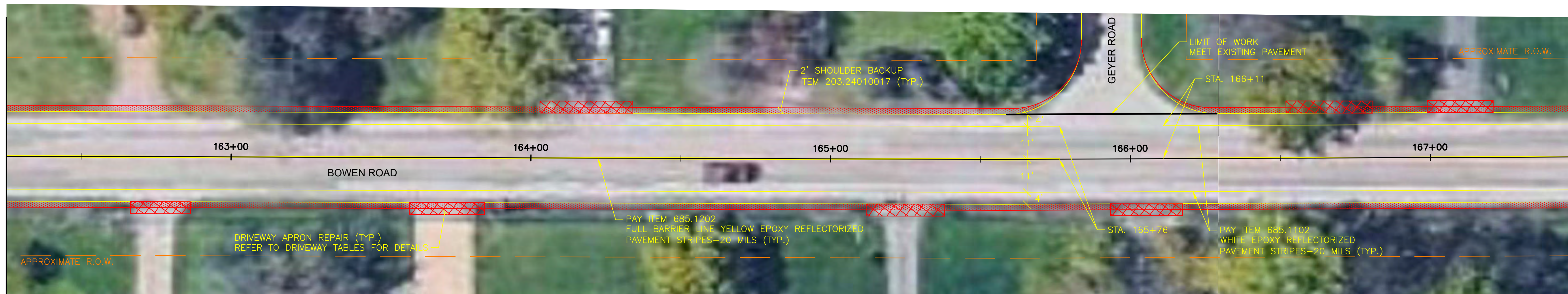
| | | |
|-------------------------|---|--------------------|
| DRAWN BY: DSJ | ERIE COUNTY DEPT. OF PUBLIC WORKS DIVISION OF HIGHWAYS 95 FRANKLIN ST BUFFALO, N.Y. | SHEET NO. 15 OF 41 |
| CHECKED BY: GMW | | DATE: MAY 26 |
| APPROVED BY: DMS | BOWEN ROAD - CR 242 MILL AND OVERLAY PLANS (10 OF 11) | SCALE: 1" = 20' |
| DATE COMPLETED: 5/12/26 | | BOP-10 |

NOTES:

- REFER TO PLANS FOR LIMITS OF STRIPING.
- ITEM 407.0102 DILUTED TACK COAT
 A. MILLED SURFACES APPLY 0.055 GALS/SY
- JOINT SEALANT (ITEM 418.7603) SHALL BE PLACED AT EACH LONGITUDINAL AND TRANSVERSE JOINT OF THE TOP COURSE.



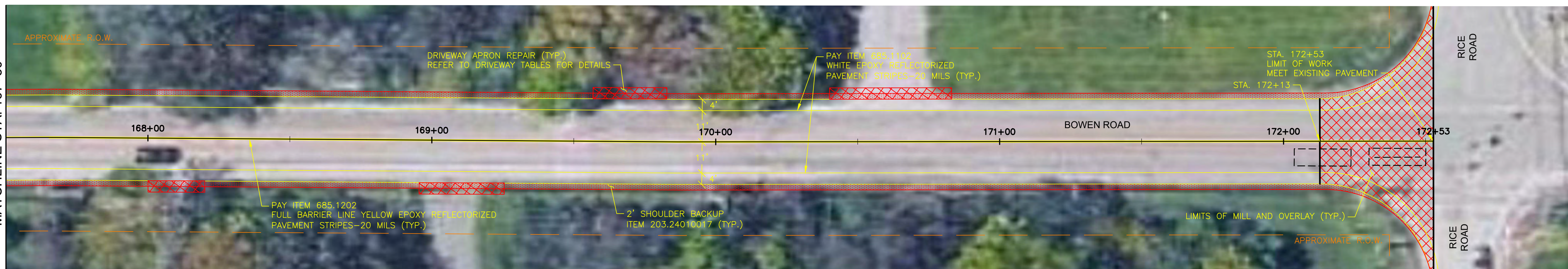
MATCHLINE STA. 162+25
 DRAWING BOP-10



PLAN
 STA. 162+25 TO STA. 167+75
 SCALE: A

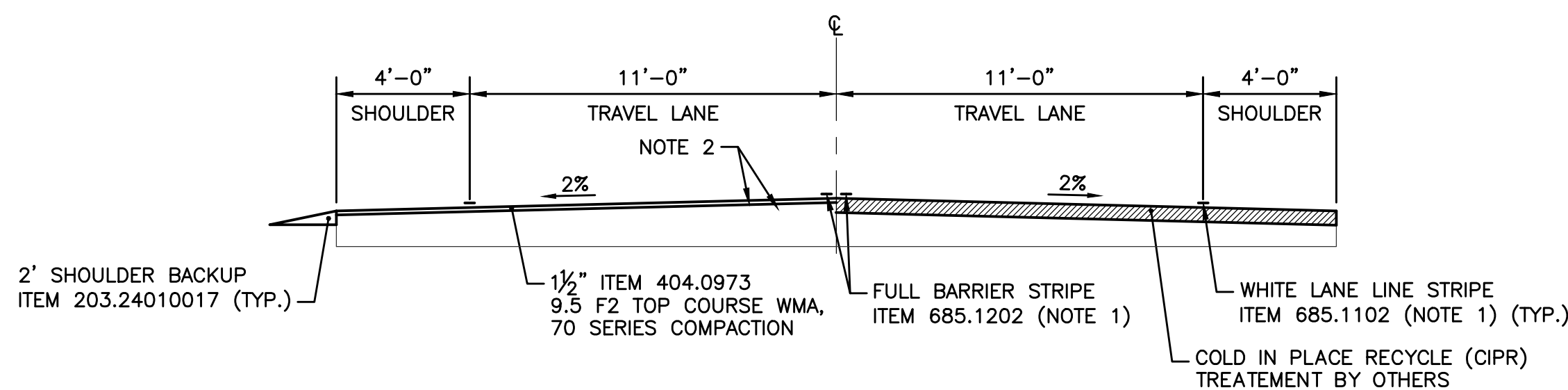
MATCHLINE STA. 167+50

MATCHLINE STA. 167+50



PLAN
 STA. 167+75 TO STA. 171+44
 SCALE: A

RICE ROAD



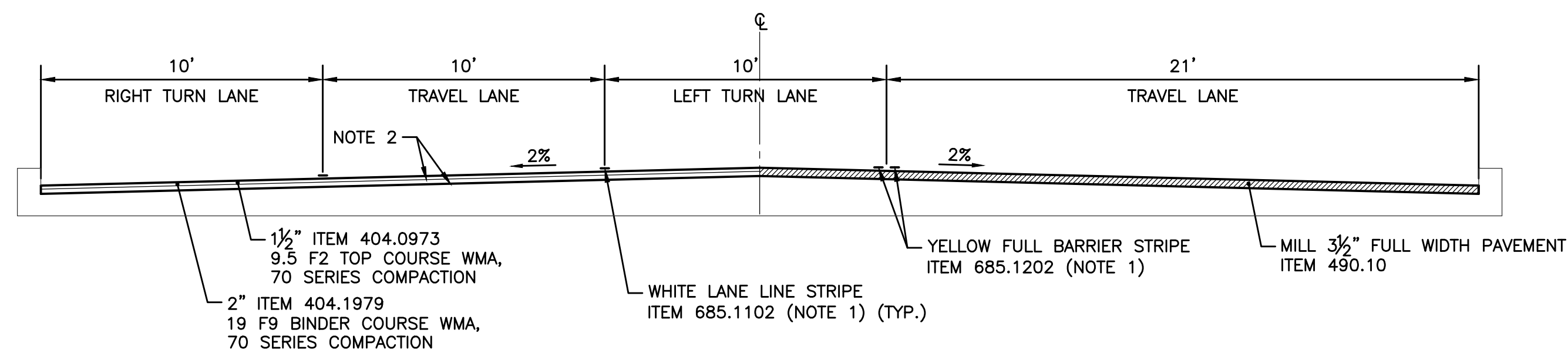
TYPICAL SECTION
 TOP COURSE OVER RECYCLED PAVEMENT
 SCALE: B



| | | |
|-------------------------|--|--------------------|
| DRAWN BY: DSJ | ERIE COUNTY DEPT. OF PUBLIC WORKS | SHEET NO. 16 OF 41 |
| CHECKED BY: GMW | DIVISION OF HIGHWAYS | DATE: MAY 26 |
| APPROVED BY: DMS | 95 FRANKLIN ST BUFFALO, N.Y. | SCALE: 1" = 20' |
| DATE COMPLETED: 5/12/26 | BOWEN ROAD - CR 242 MILL AND OVERLAY PLANS (11 OF 11) | BOP-11 |

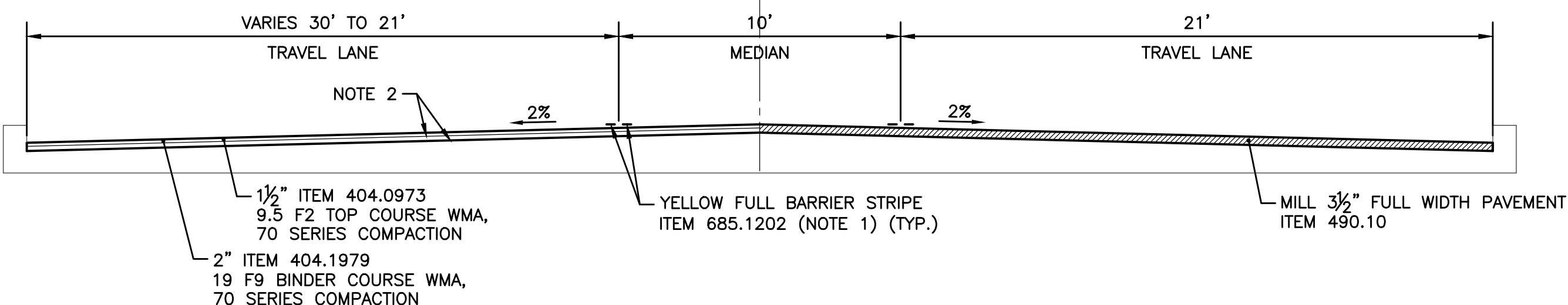
NOTES:

- REFER TO PLANS FOR LIMITS OF STRIPING.
- ITEM 407.0102 DILUTED TACK COAT
A. MILLED SURFACES APPLY 0.055 GALS/SY
B. BETWEEN COURSES APPLY 0.035 GALS/SY
- JOINT SEALANT (ITEM 418.7603) SHALL BE PLACED AT EACH LONGITUDINAL AND TRANSVERSE JOINT OF THE TOP COURSE.



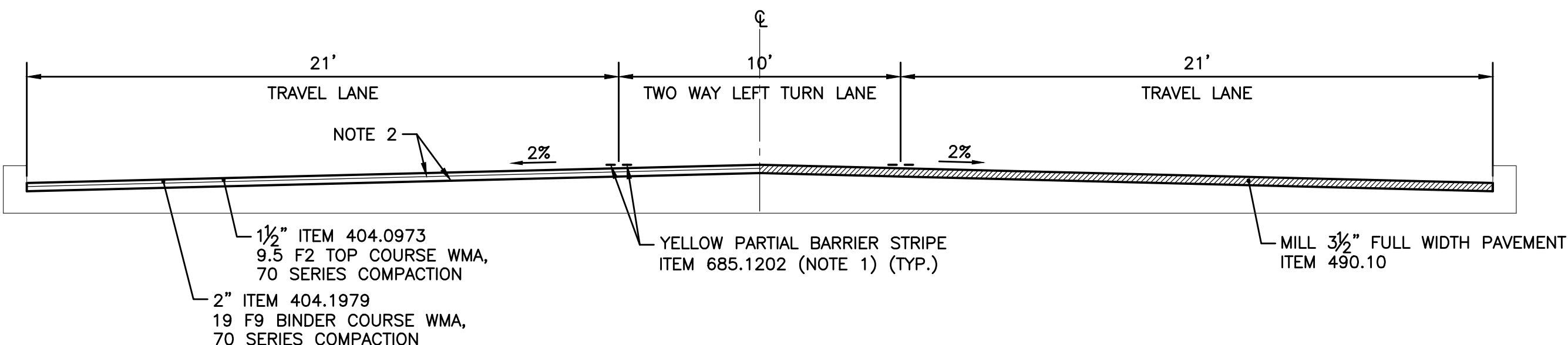
TYPICAL SECTION

STA. 0+90 TO STA. 2+10
SCALE: A



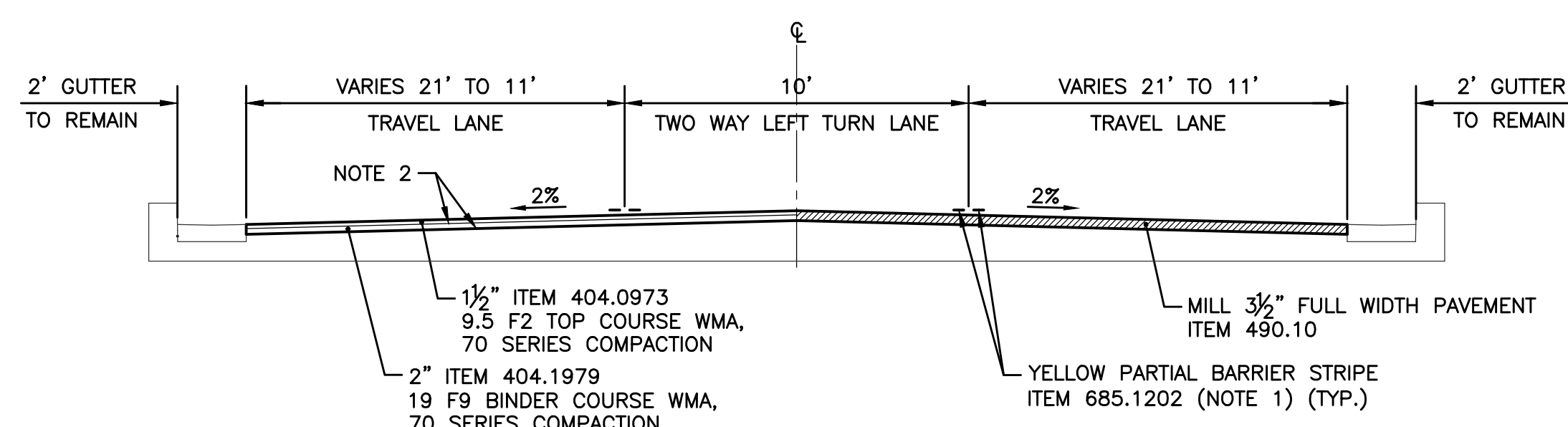
TYPICAL SECTION

STA. 2+10 TO STA. 3+60
SCALE: A



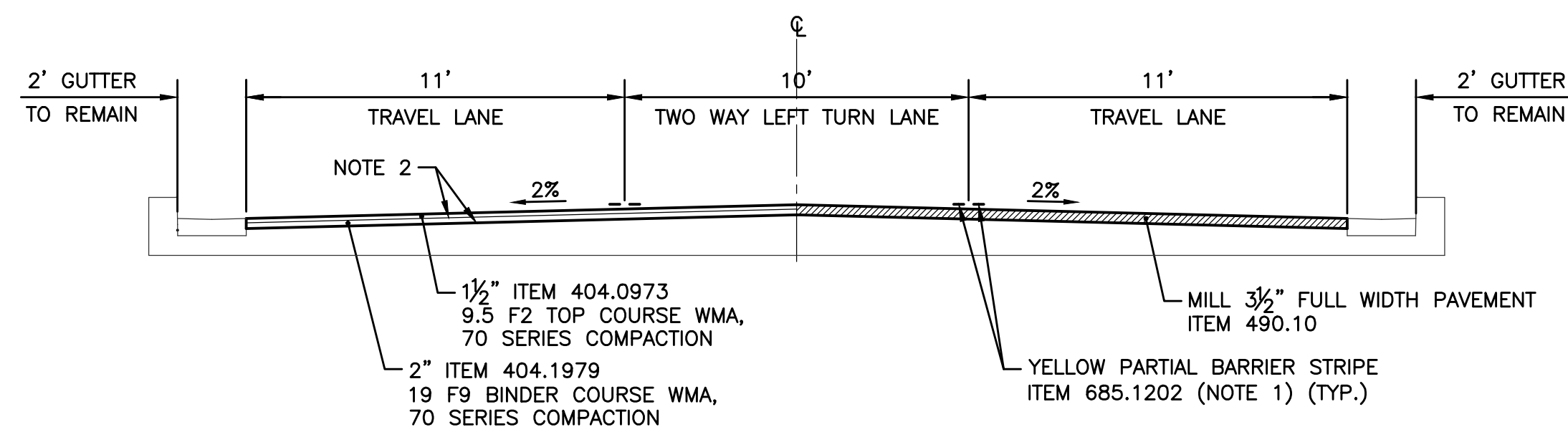
TYPICAL SECTION

STA. 3+60 TO STA. 4+77
SCALE: A



TYPICAL SECTION

STA. 4+77 TO STA. 7+93
SCALE: A

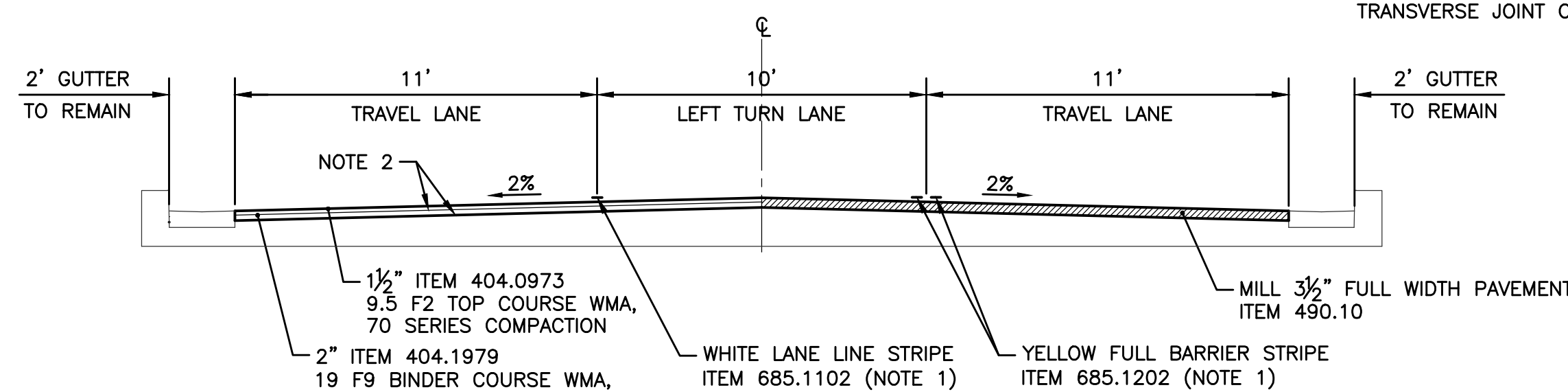


TYPICAL SECTION

STA. 7+93 TO STA. 14+44
STA. 16+89 TO STA. 18+11
STA. 20+46 TO STA. 21+33
STA. 22+81 TO STA. 26+61
STA. 28+97 TO STA. 30+36

STA. 32+66 TO STA. 40+26
STA. 46+01 TO STA. 51+12
STA. 53+49 TO STA. 58+46
STA. 73+18 TO STA. 80+25

SCALE: A

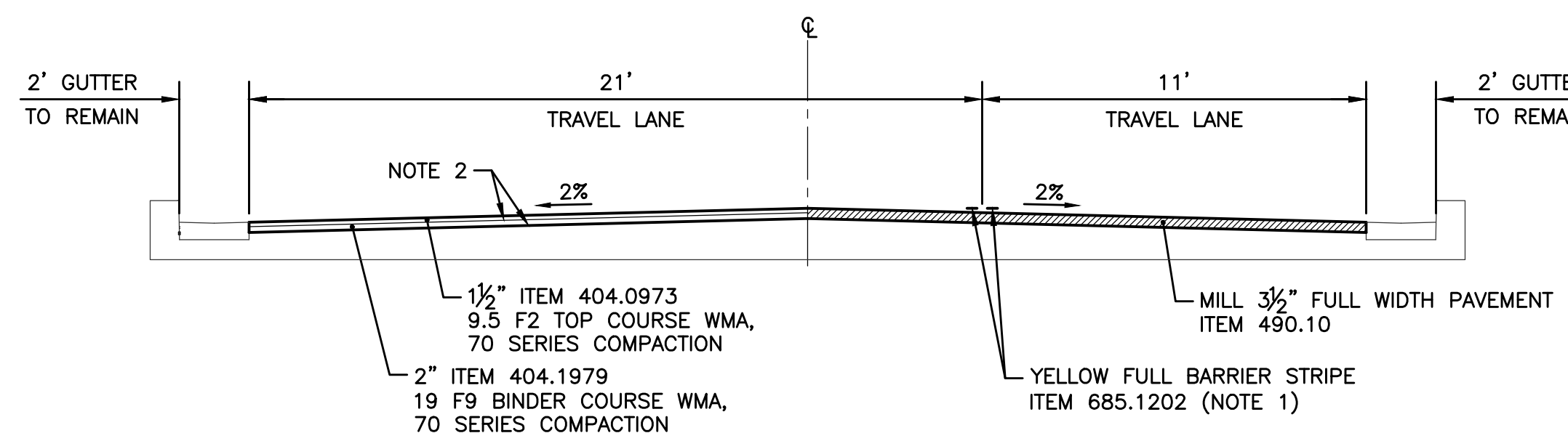


TYPICAL SECTION

STA. 14+44 TO STA. 16+39
STA. 18+11 TO STA. 19+96
STA. 21+33 TO STA. 23+31
STA. 26+61 TO STA. 28+47
STA. 30+36 TO STA. 32+16

STA. 40+26 TO STA. 42+23
STA. 43+54 TO STA. 45+51
STA. 51+12 TO STA. 52+99
STA. 58+46 TO STA. 72+68

SCALE: A

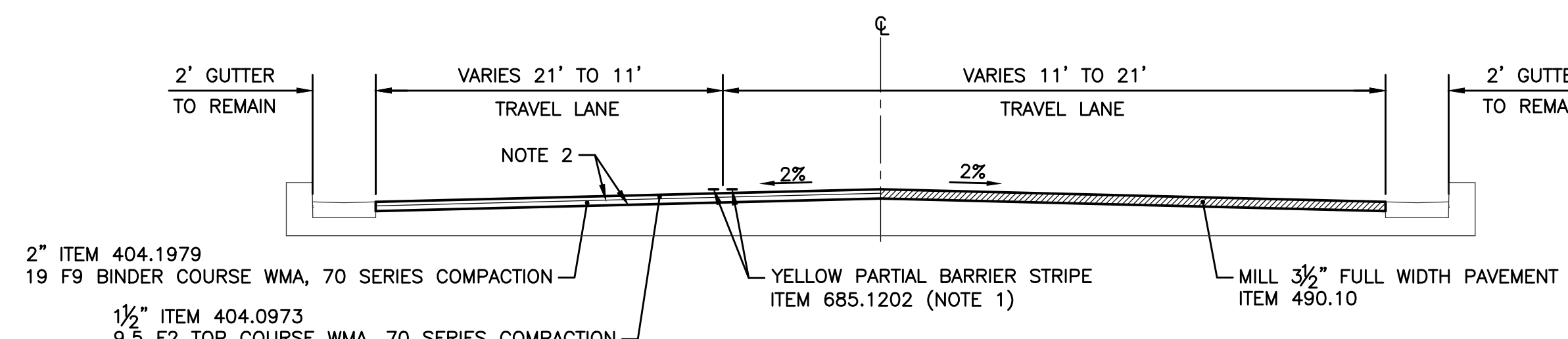


TYPICAL SECTION

STA. 16+39 TO STA. 16+89
STA. 19+96 TO STA. 20+46
STA. 23+31 TO STA. 23+81
STA. 28+47 TO STA. 28+97

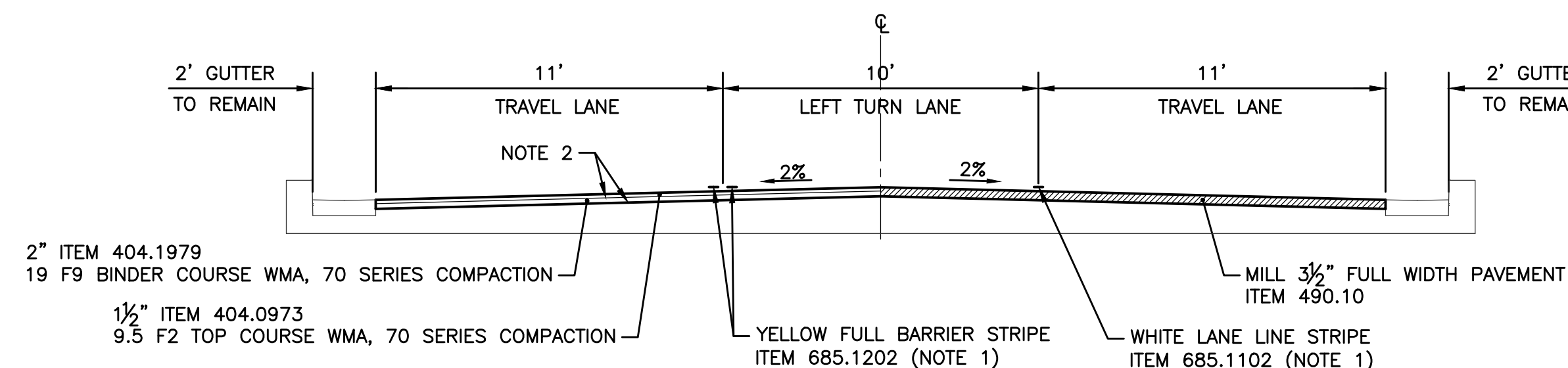
STA. 32+16 TO STA. 32+66
STA. 45+51 TO STA. 46+01
STA. 52+99 TO STA. 53+49
STA. 72+68 TO STA. 73+18

SCALE: A



TYPICAL SECTION

STA. 16+89 TO STA. 19+96
SCALE: A



TYPICAL SECTION

STA. 19+96 TO STA. 20+46
SCALE: A

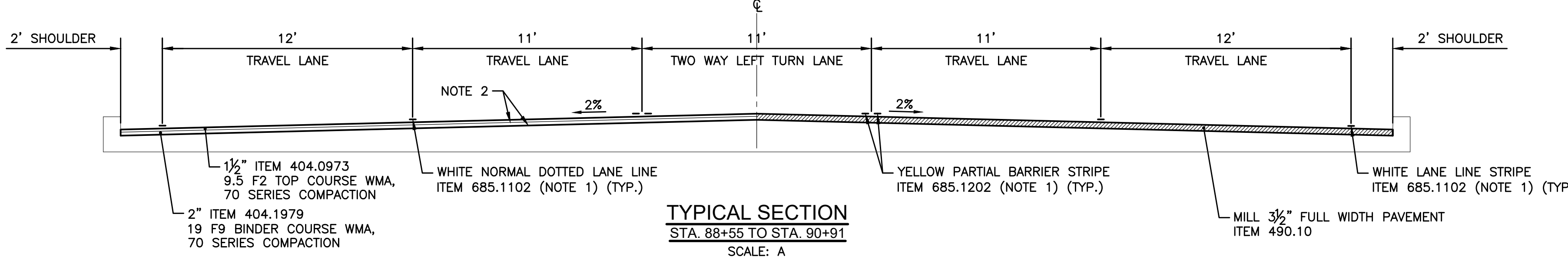
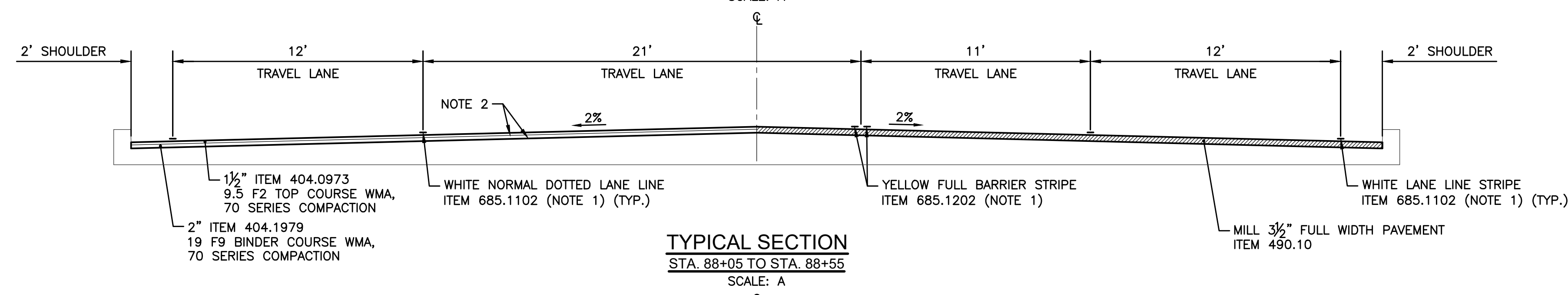
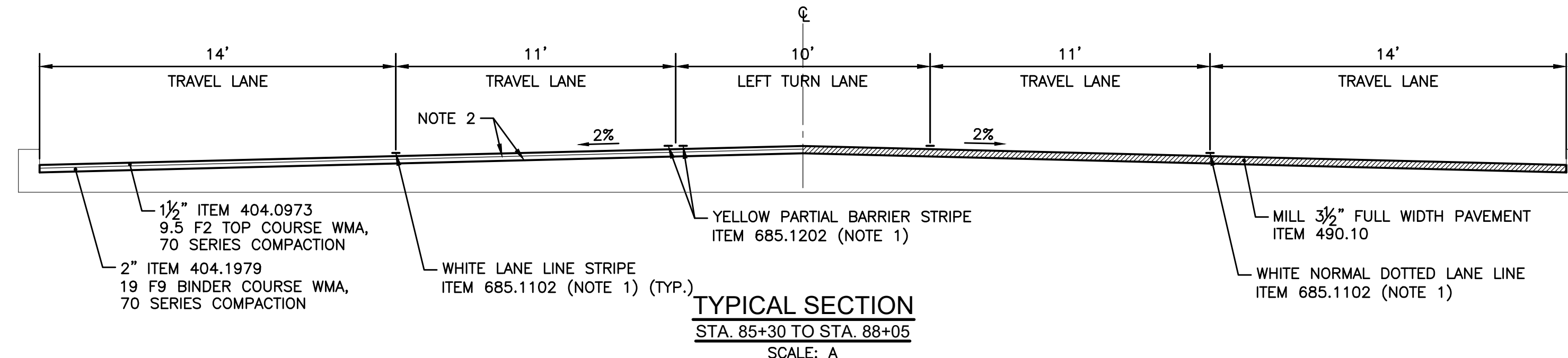
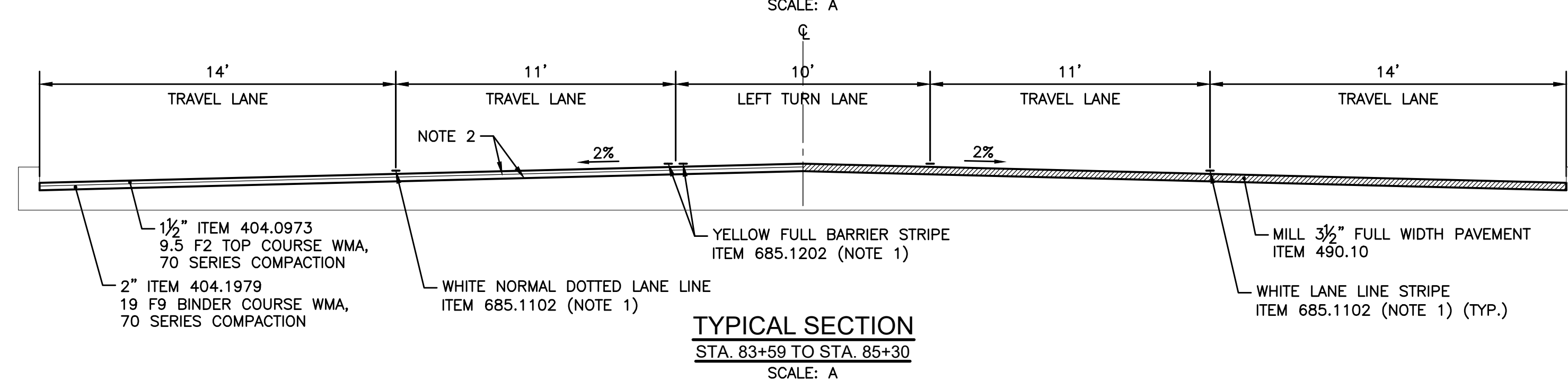
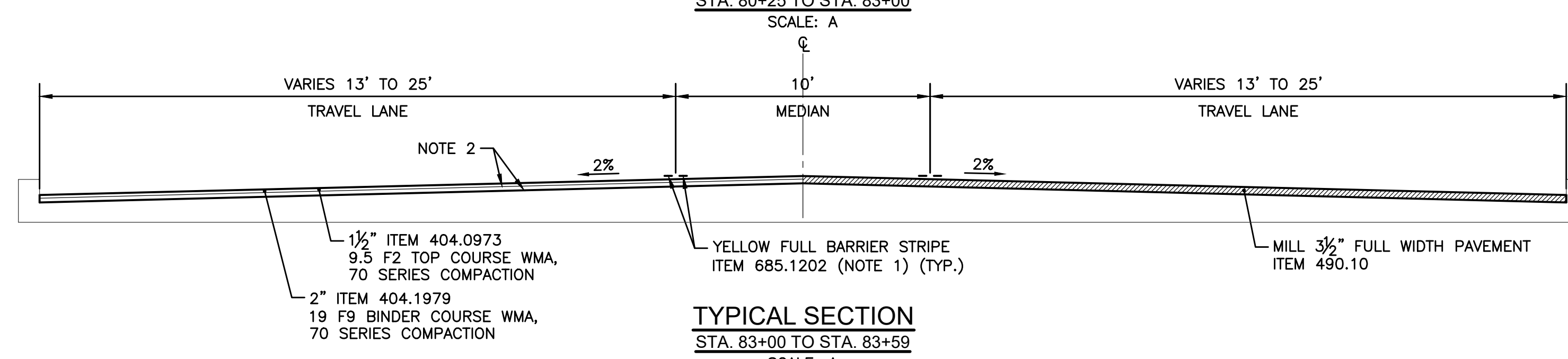
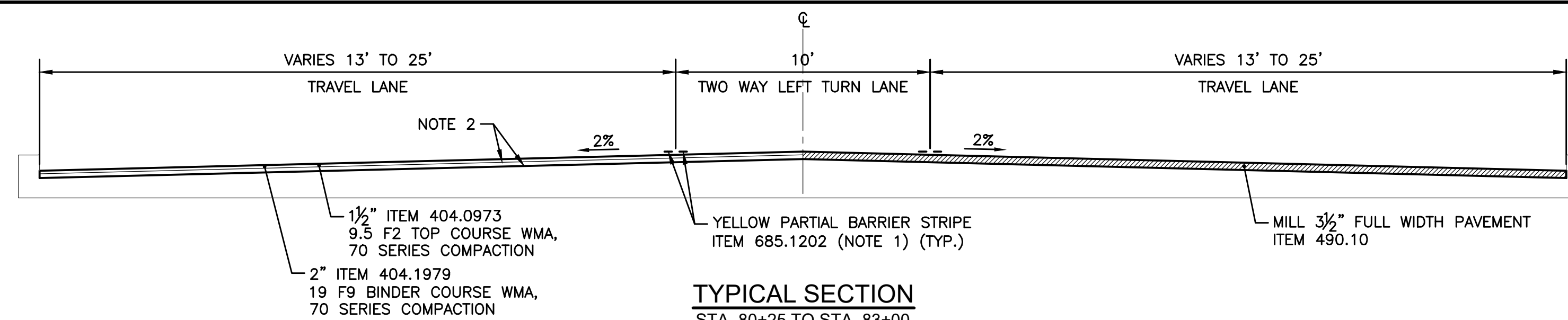


SCALE A

| | | |
|-------------------------|-----------------------------------|--------------------|
| DRAWN BY: DSJ | ERIE COUNTY DEPT. OF PUBLIC WORKS | SHEET NO. 17 OF 41 |
| CHECKED BY: GMW | DIVISION OF HIGHWAYS | DATE: MAY 26 |
| APPROVED BY: DMS | 95 FRANKLIN ST. BUFFALO, N.Y. | SCALE: 1"=4' |
| DATE COMPLETED: 5/12/26 | NORTH FOREST ROAD - CR 294 | NFS-1 |
| | TYPICAL SECTIONS (1 OF 3) | |

NOTES:

1. REFER TO PLANS FOR LIMITS OF STRIPING.
2. ITEM 407.0102 DILUTED TACK COAT
 - A. MILLED SURFACES APPLY 0.055 GALS/SY
 - B. BETWEEN COURSES APPLY 0.035 GALS/SY
3. JOINT SEALANT (ITEM 418.7603) SHALL BE PLACED AT EACH LONGITUDINAL AND TRANSVERSE JOINT OF THE TOP COURSE.

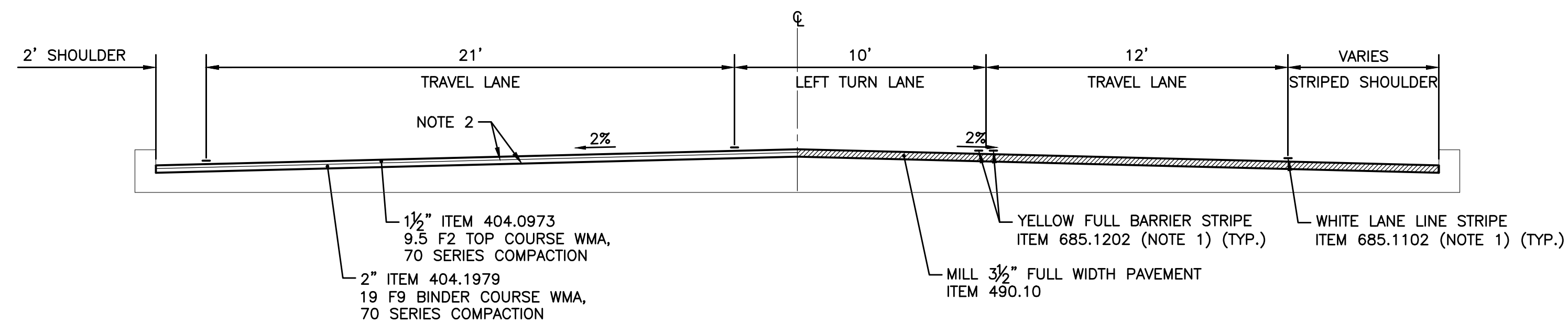


| | | |
|-------------------------|-----------------------------------|--------------------|
| DRAWN BY: DSU | ERIE COUNTY DEPT. OF PUBLIC WORKS | SHEET NO. 18 OF 41 |
| CHECKED BY: GMW | DIVISION OF HIGHWAYS | DATE: MAY 26 |
| APPROVED BY: DMS | 95 FRANKLIN ST BUFFALO, N.Y. | SCALE: 1"=4' |
| DATE COMPLETED: 5/12/26 | NORTH FOREST ROAD - CR 294 | NFS-2 |
| | TYPICAL SECTIONS (2 OF 3) | |

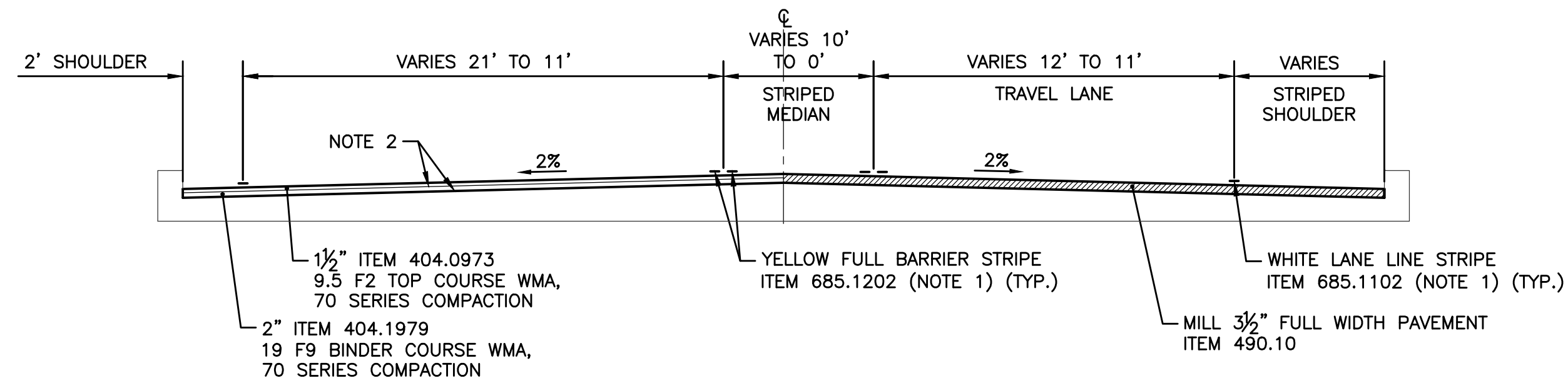
V:\Public Works\Highways - Main Road Files\Overlays\2026 Overlay Program\NorthForest

NOTES:

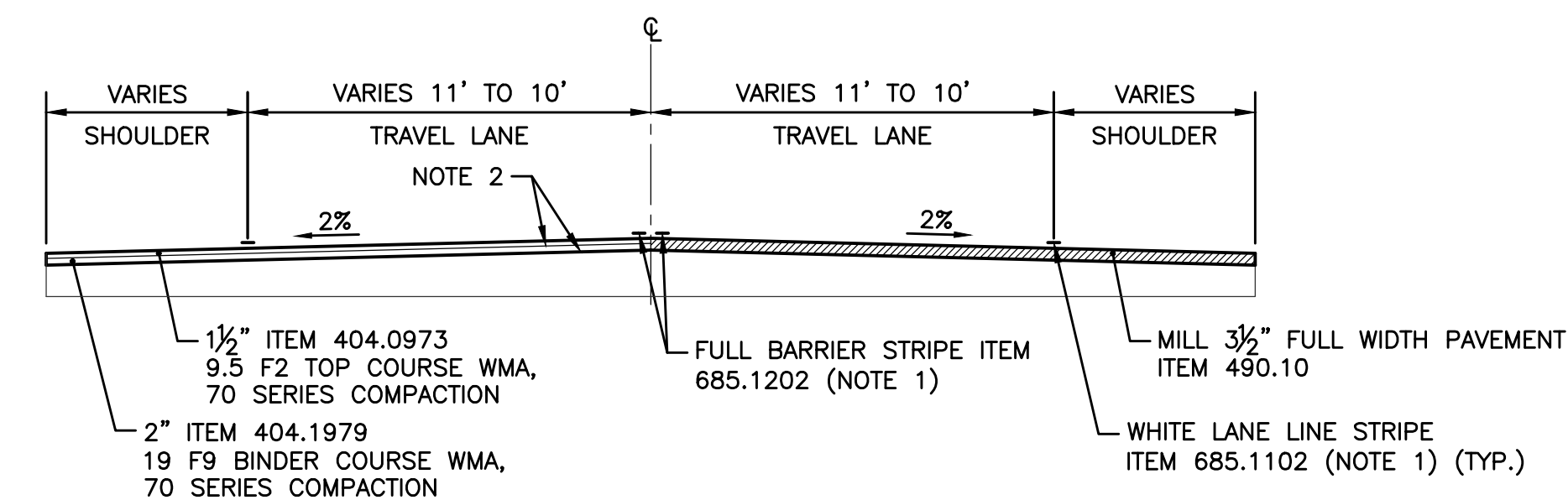
- REFER TO PLANS FOR LIMITS OF STRIPING.
- ITEM 407.0102 DILUTED TACK COAT
 - MILLED SURFACES APPLY 0.055 GALS/SY
 - BETWEEN COURSES APPLY 0.035 GALS/SY
- JOINT SEALANT (ITEM 418.7603) SHALL BE PLACED AT EACH LONGITUDINAL AND TRANSVERSE JOINT OF THE TOP COURSE.



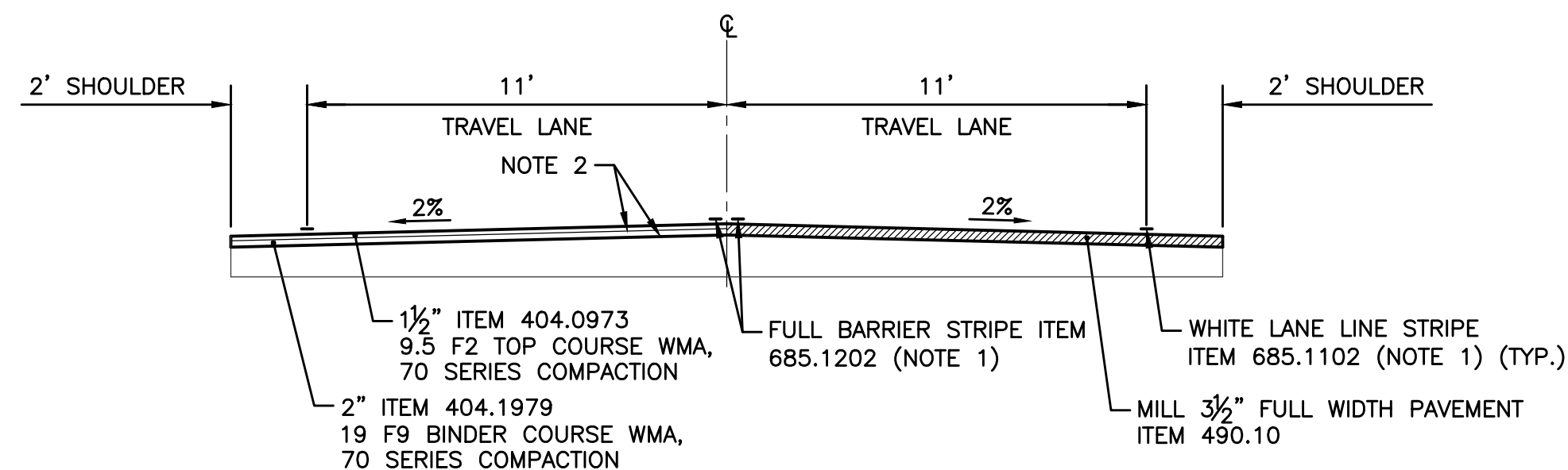
TYPICAL SECTION
 STA. 90+91 TO STA. 91+86
 SCALE: A



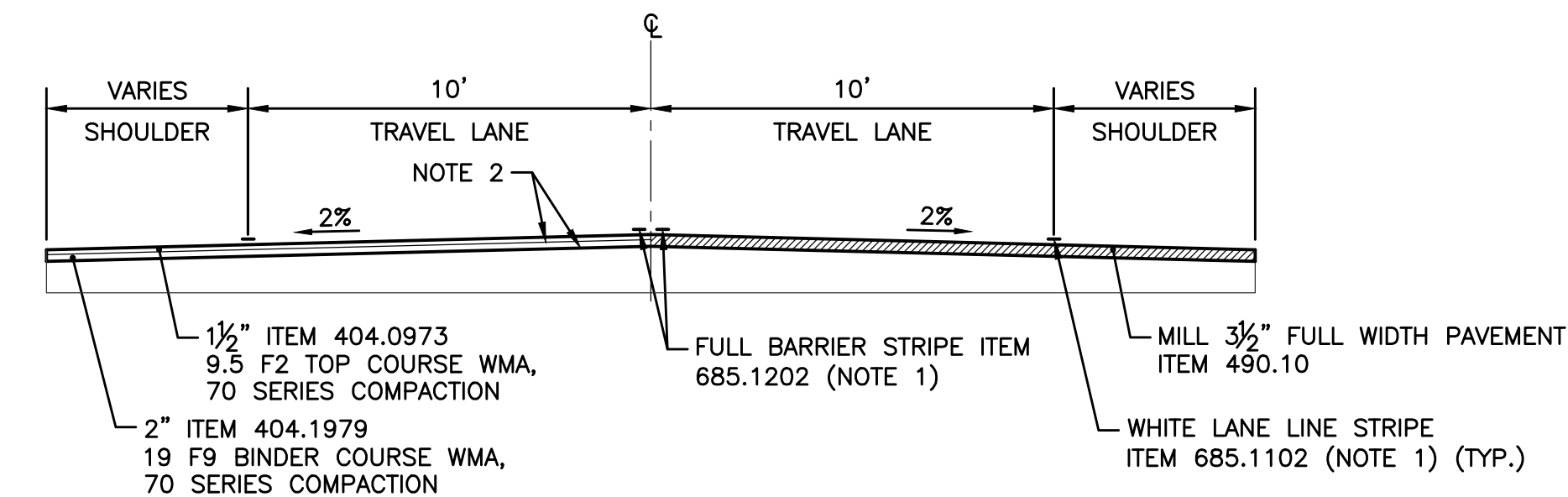
TYPICAL SECTION
 STA. 91+86 TO STA. 93+68
 SCALE: A



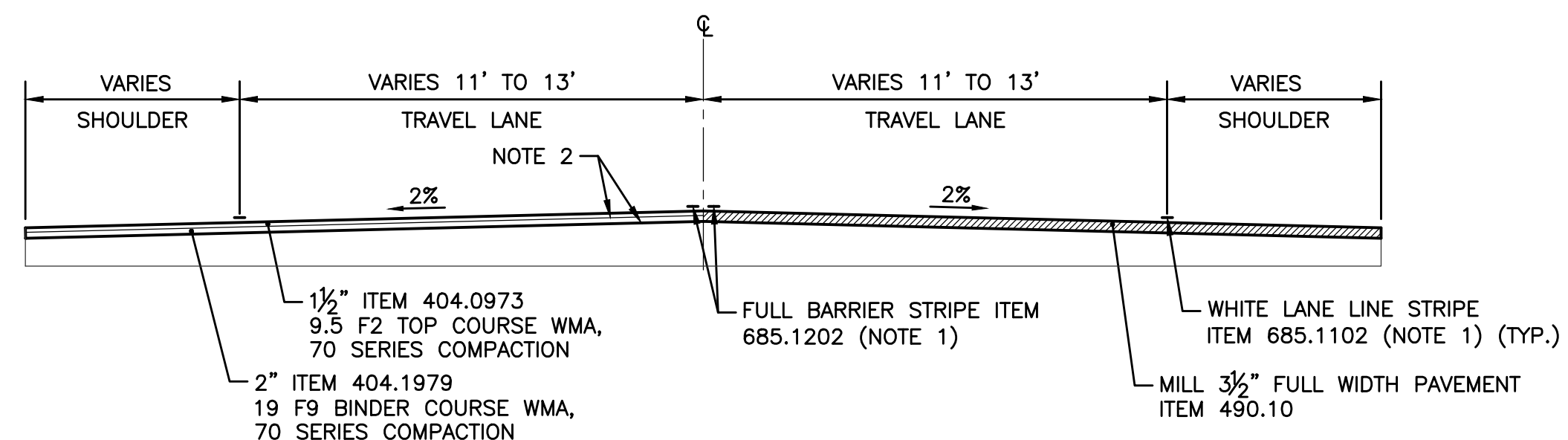
TYPICAL SECTION
 STA. 102+69 TO STA. 105+87
 SCALE: A



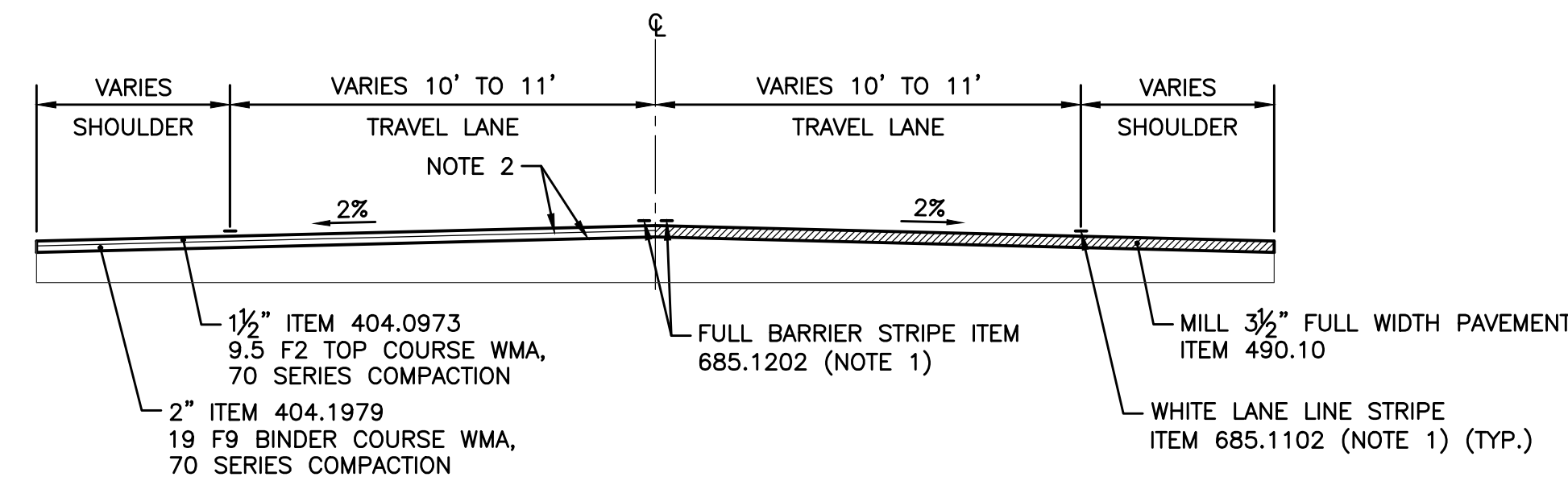
TYPICAL SECTION
 STA. 93+68 TO STA. 100+07
 SCALE: A



TYPICAL SECTION
 STA. 105+87 TO STA. 167+00
 SCALE: A



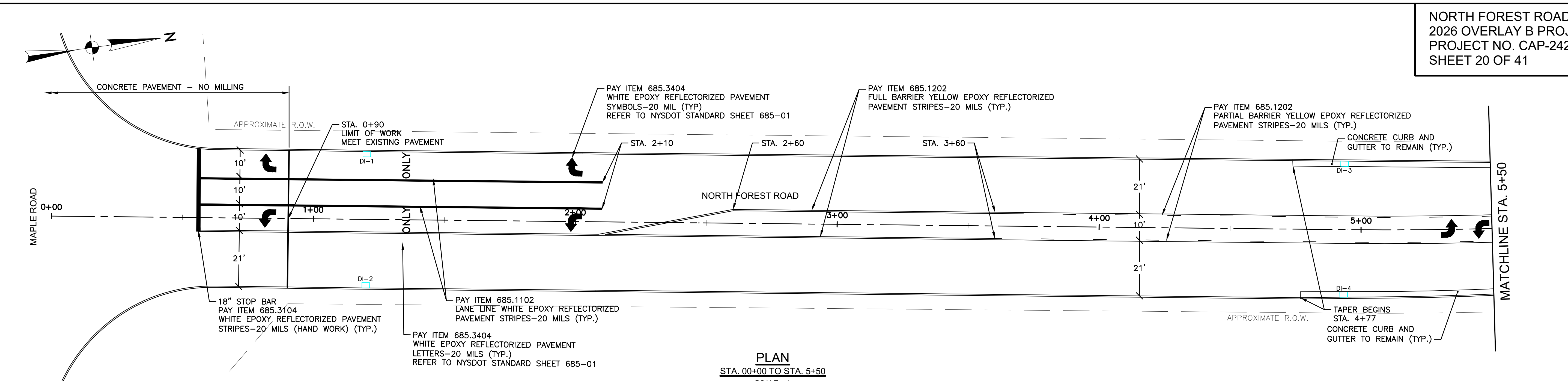
TYPICAL SECTION
 STA. 100+07 TO STA. 102+69
 SCALE: A



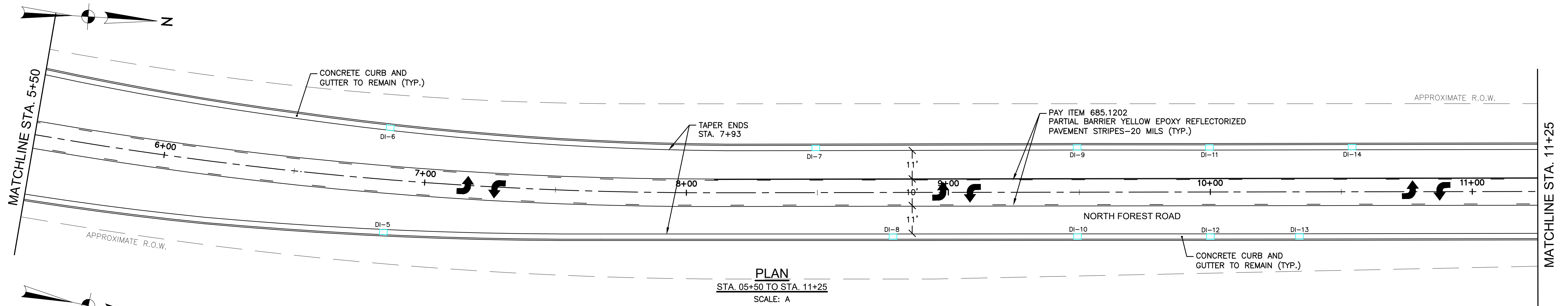
TYPICAL SECTION
 STA. 167+00 TO STA. 173+50
 SCALE: A



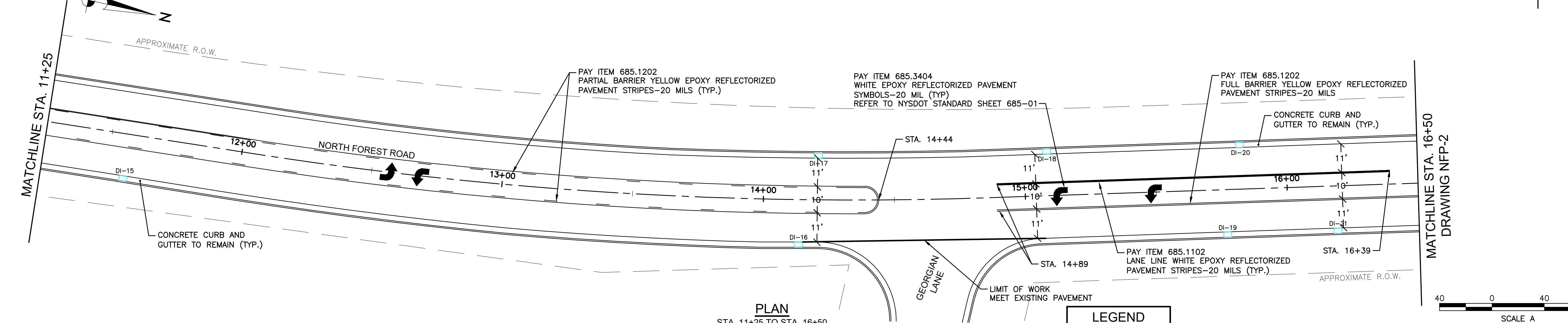
| | | |
|-------------------------|-----------------------------------|--------------------|
| DRAWN BY: DSJ | ERIE COUNTY DEPT. OF PUBLIC WORKS | SHEET NO. 19 OF 41 |
| CHECKED BY: GMW | DIVISION OF HIGHWAYS | DATE: MAY 26 |
| APPROVED BY: DMS | 95 FRANKLIN ST. BUFFALO, N.Y. | SCALE: 1"=4' |
| DATE COMPLETED: 5/12/26 | NORTH FOREST ROAD - CR 294 | NFS-3 |
| | TYPICAL SECTIONS (3 OF 3) | |



PLAN
 STA. 00+00 TO STA. 5+50
 SCALE: A



PLAN
 STA. 05+50 TO STA. 11+25
 SCALE: A

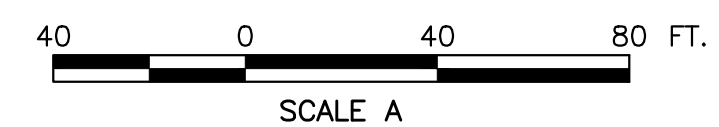


PLAN
 STA. 11+25 TO STA. 16+50
 SCALE: A

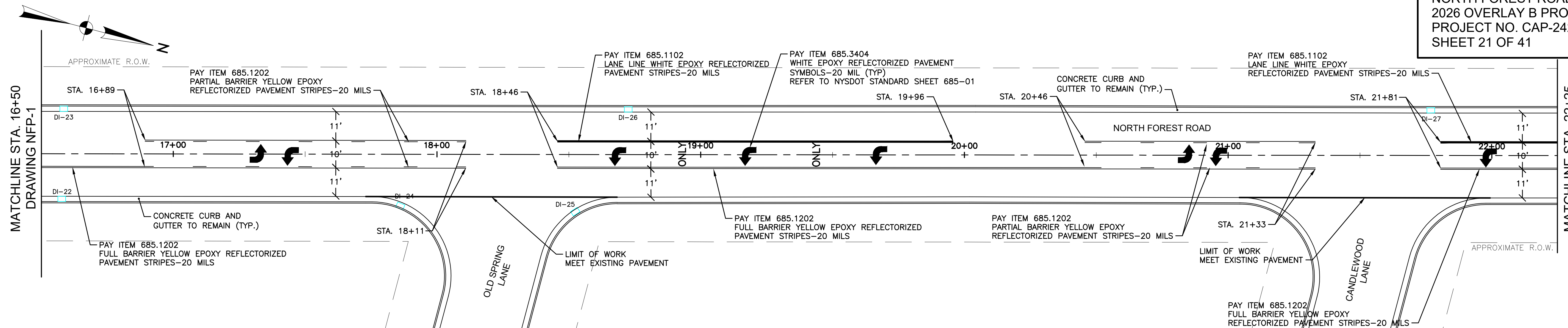
LEGEND

- SEWER MANHOLE
- WATER VALVE
- DRAINAGE INLET

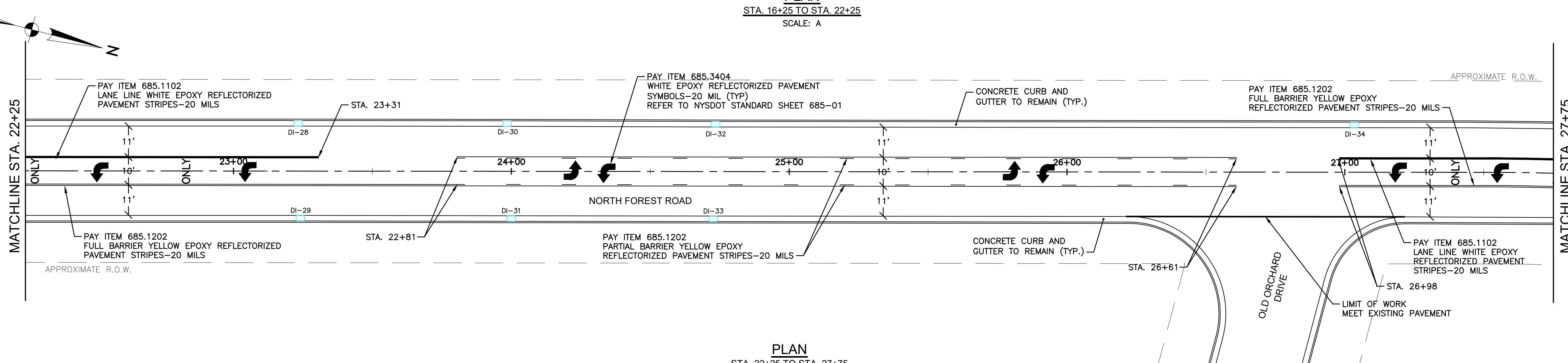
| | | |
|-------------------------|-----------------------------------|--------------------|
| DRAWN BY: DSJ | ERIE COUNTY DEPT. OF PUBLIC WORKS | SHEET NO. 20 OF 41 |
| CHECKED BY: GMW | DIVISION OF HIGHWAYS | DATE: MAY 26 |
| APPROVED BY: DMS | 95 FRANKLIN ST. BUFFALO, N.Y. | SCALE: 1" = 20' |
| DATE COMPLETED: 5/12/26 | NORTH FOREST ROAD - CR 294 | NFP-1 |
| | MILL AND OVERLAY PLANS (1 OF 12) | |



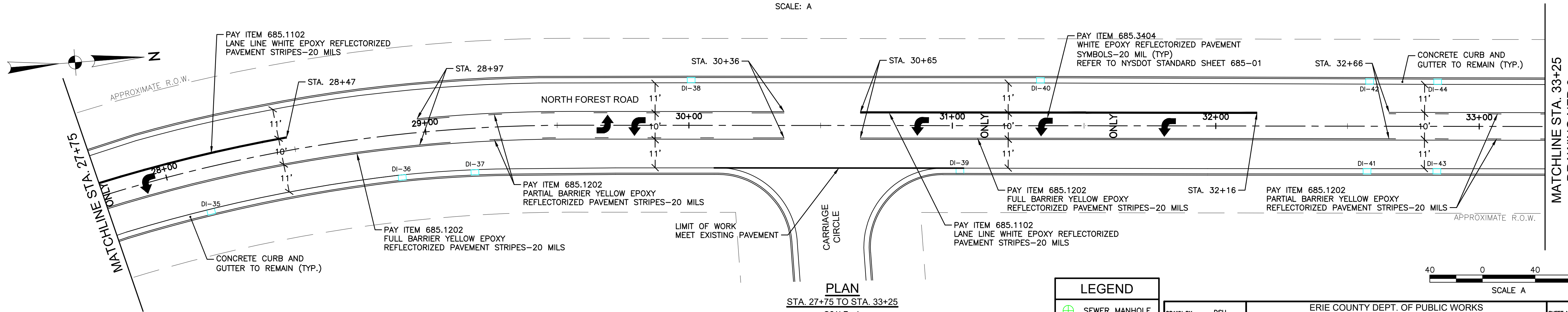
V:\Public Works\Highways - Main Road Files\Overlays\2026 Overlay Program\NorthForest



PLAN
 STA. 16+25 TO STA. 22+25
 SCALE: A



PLAN
 STA. 22+25 TO STA. 27+75
 SCALE: A



PLAN
 STA. 27+75 TO STA. 33+25
 SCALE: A

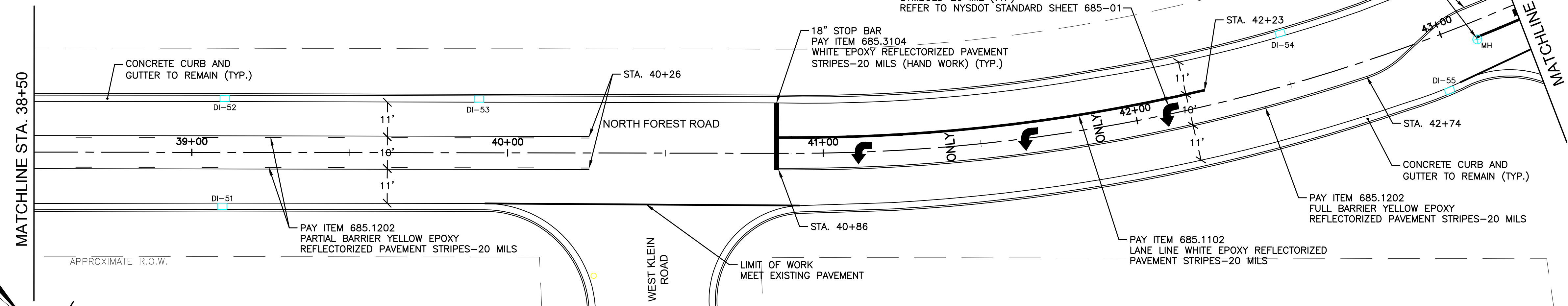
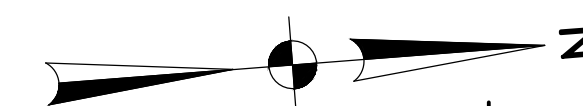
| LEGEND | |
|--------|----------------|
| | SEWER MANHOLE |
| | WATER VALVE |
| | DRAINAGE INLET |



| | | |
|-------------------------|-----------------------------------|--------------------|
| DRAWN BY: DSJ | ERIE COUNTY DEPT. OF PUBLIC WORKS | SHEET NO. 21 OF 41 |
| CHECKED BY: GMW | DIVISION OF HIGHWAYS | DATE: MAY 26 |
| APPROVED BY: DMS | 95 FRANKLIN ST. BUFFALO, N.Y. | SCALE: 1" = 20' |
| DATE COMPLETED: 5/12/26 | NORTH FOREST ROAD - CR 294 | NFP-2 |
| | MILL AND OVERLAY PLANS (2 OF 12) | |

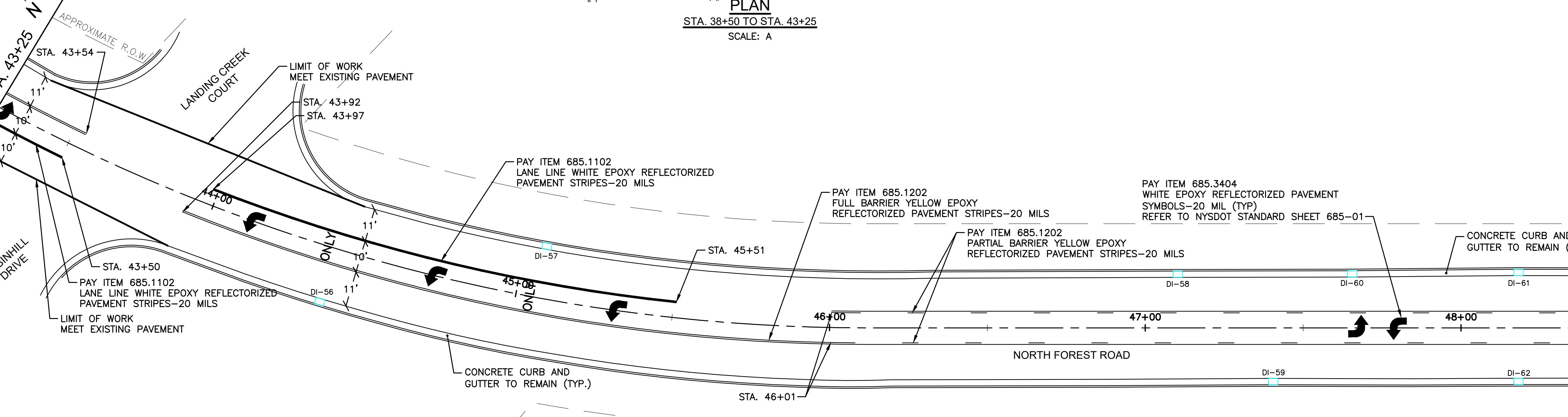
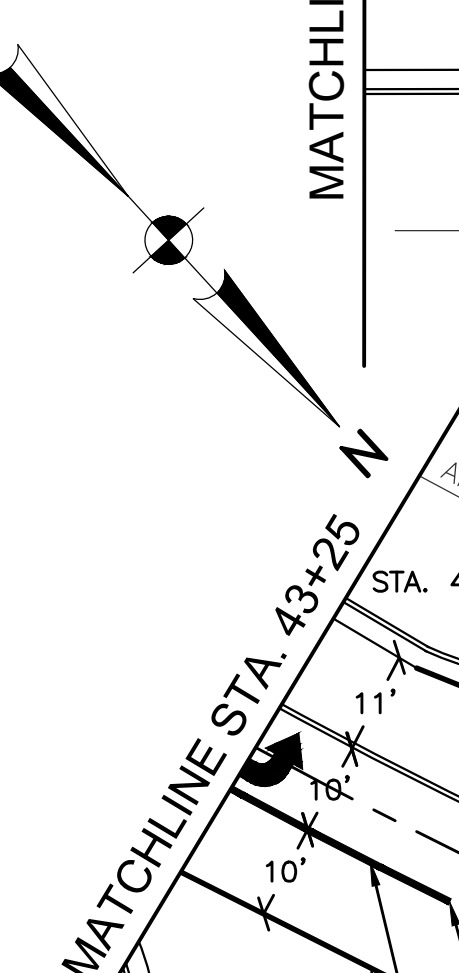
MATCHLINE STA. 33+25
 DRAWING NFP-2

MATCHLINE STA. 38+50



PLAN
 STA. 33+25 TO STA. 38+50
 SCALE: A

PLAN
 STA. 38+50 TO STA. 43+25
 SCALE: A



PLAN
 STA. 43+25 TO STA. 48+50
 SCALE: A

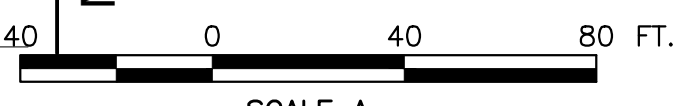
MATCHLINE STA. 48+50
 DRAWING EGP-4

| LEGEND | |
|--------|----------------|
| | SEWER MANHOLE |
| | WATER VALVE |
| | DRAINAGE INLET |

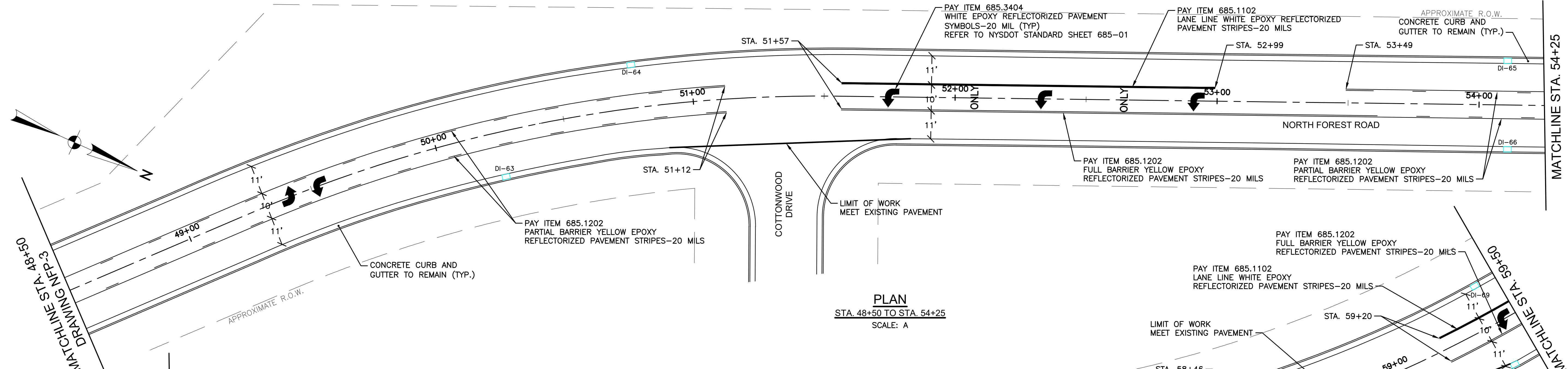
| | |
|----------------|---------|
| DRAWN BY | DSU |
| CHECKED BY | GMW |
| APPROVED BY | DMS |
| DATE COMPLETED | 5/12/26 |

| | |
|-----------------------------------|---------------|
| ERIE COUNTY DEPT. OF PUBLIC WORKS | |
| DIVISION OF HIGHWAYS | |
| 95 FRANKLIN ST | BUFFALO, N.Y. |
| NORTH FOREST ROAD - CR 294 | |
| MILL AND OVERLAY PLANS (3 OF 12) | |

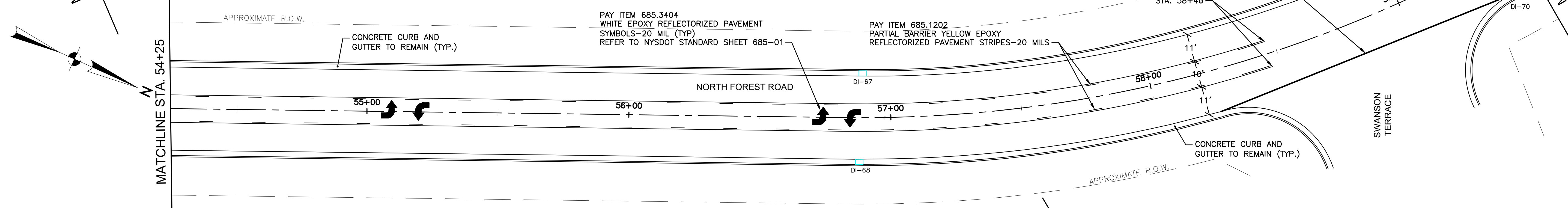
| | |
|-----------|----------|
| SHEET NO. | 22 OF 41 |
| DATE | MAY 26 |
| SCALE | 1" = 20' |
| | NFP-3 |



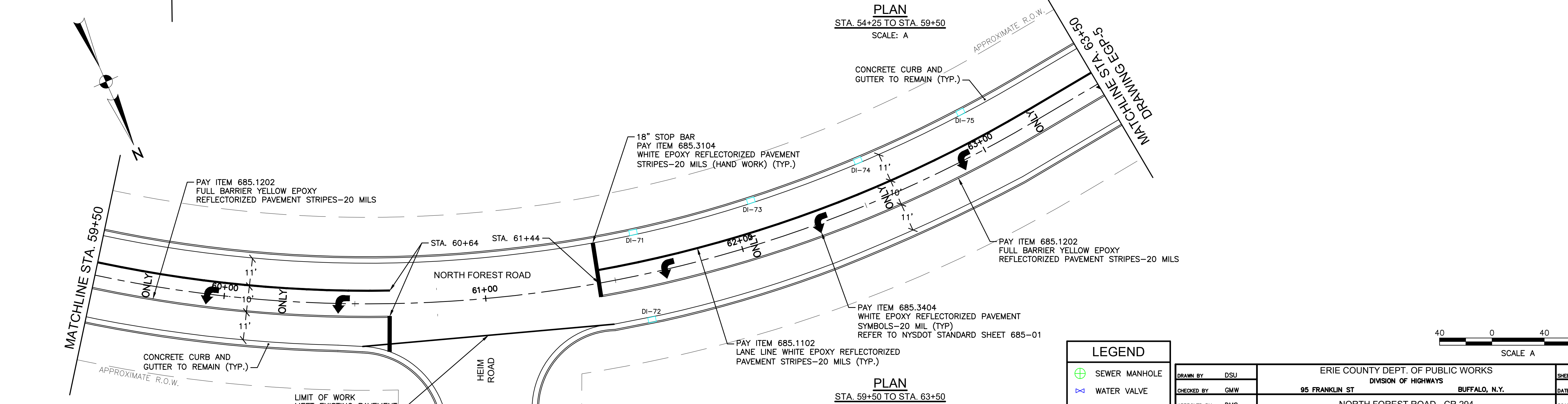
V:\Public Works\Highways - Main Road Files\Overlays\2026 Overlay Program\NorthForest



PLAN
 STA. 48+50 TO STA. 54+25
 SCALE: A



PLAN
 STA. 54+25 TO STA. 59+50
 SCALE: A

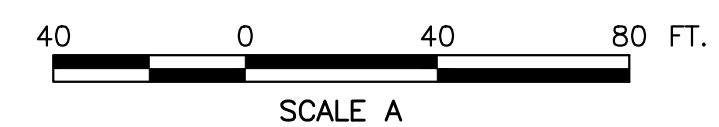


PLAN
 STA. 59+50 TO STA. 63+50
 SCALE: A

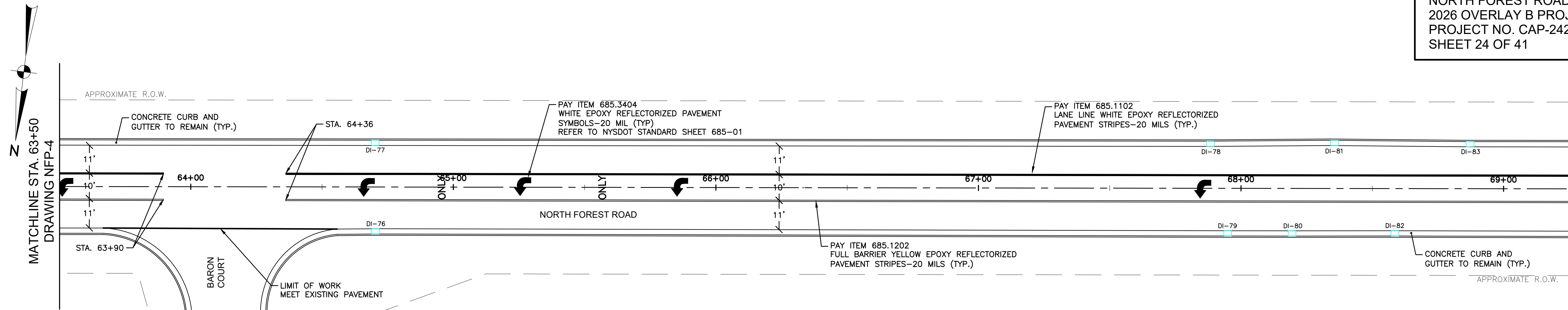
LEGEND

| | |
|--|----------------|
| | SEWER MANHOLE |
| | WATER VALVE |
| | DRAINAGE INLET |

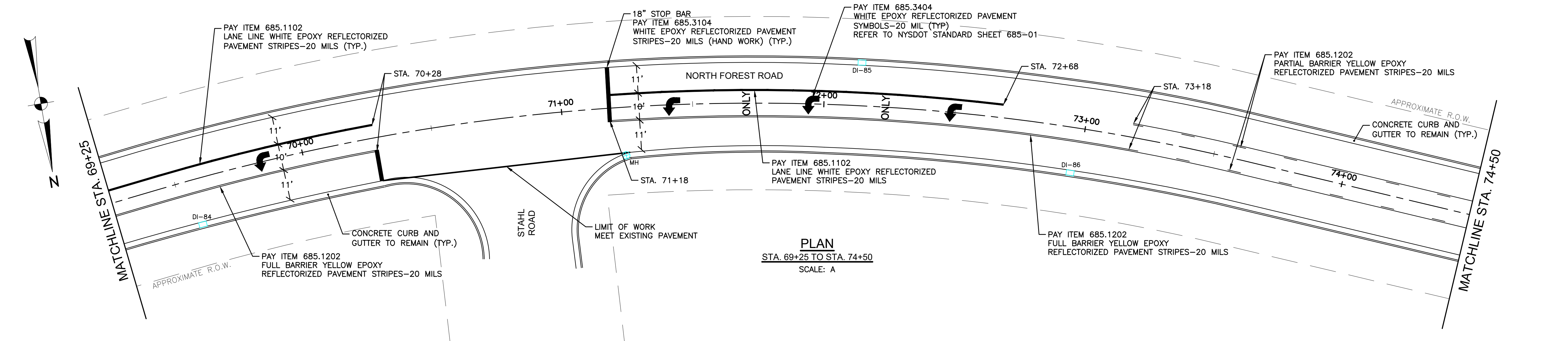
| | | |
|-------------------------|--|--------------------|
| DRAWN BY: DSJ | ERIE COUNTY DEPT. OF PUBLIC WORKS | SHEET NO. 23 OF 41 |
| CHECKED BY: GMW | DIVISION OF HIGHWAYS | DATE: MAY 26 |
| APPROVED BY: DMS | 95 FRANKLIN ST. BUFFALO, N.Y. | SCALE: 1" = 20' |
| DATE COMPLETED: 5/12/26 | NORTH FOREST ROAD - CR 294 MILL AND OVERLAY PLANS (4 OF 12) | NFP-4 |



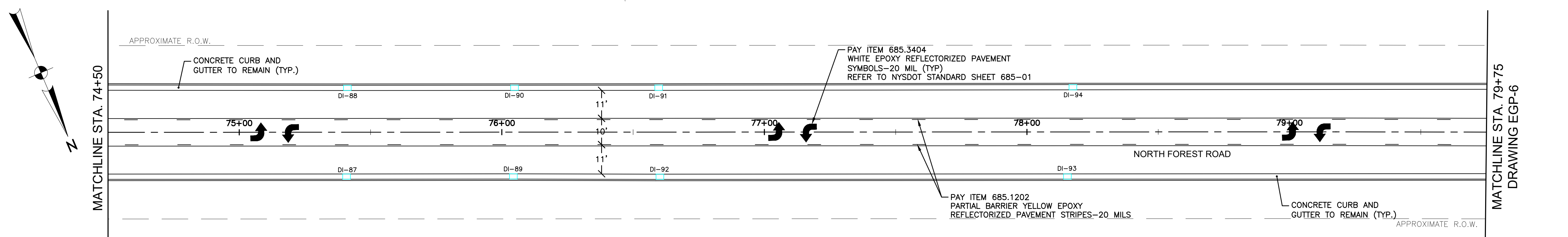
V:\Public Works\Highways - Main\Road Files\Overlays\2026 Overlay Program\NorthForest



PLAN
 STA. 63+50 TO STA. 69+25
 SCALE: A



PLAN
 STA. 69+25 TO STA. 74+50
 SCALE: A

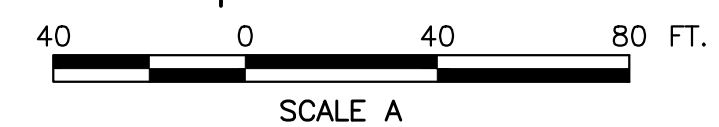


PLAN
 STA. 74+50 TO STA. 79+75
 SCALE: A

LEGEND

- SEWER MANHOLE
- WATER VALVE
- DRAINAGE INLET

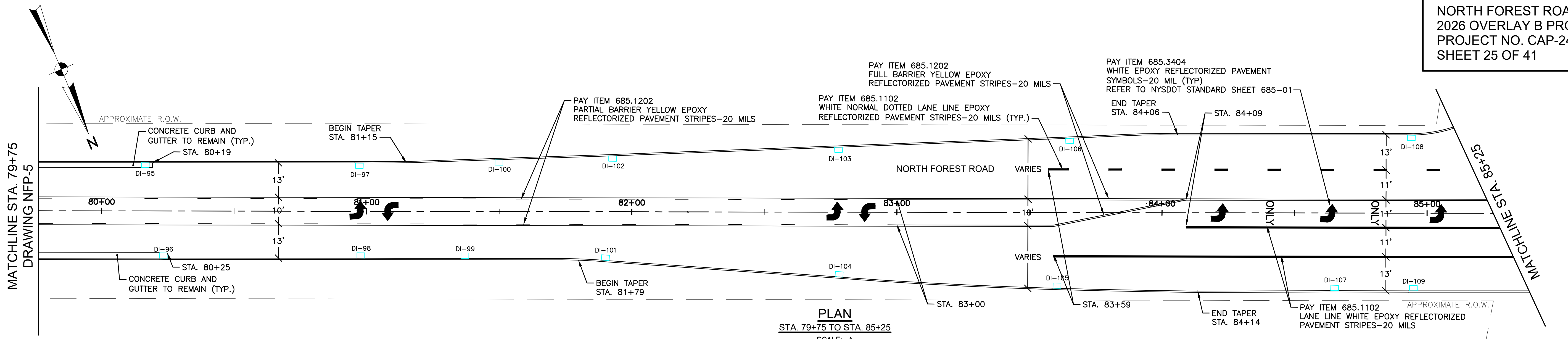
| | | |
|-------------------------|-----------------------------------|--------------------|
| DRAWN BY: DSJ | ERIE COUNTY DEPT. OF PUBLIC WORKS | SHEET NO. 24 OF 41 |
| CHECKED BY: GMW | DIVISION OF HIGHWAYS | DATE: MAY 26 |
| APPROVED BY: DMS | 95 FRANKLIN ST. BUFFALO, N.Y. | SCALE: 1" = 20' |
| DATE COMPLETED: 5/12/26 | NORTH FOREST ROAD - CR 294 | NFP-5 |
| | MILL AND OVERLAY PLANS (5 OF 12) | |



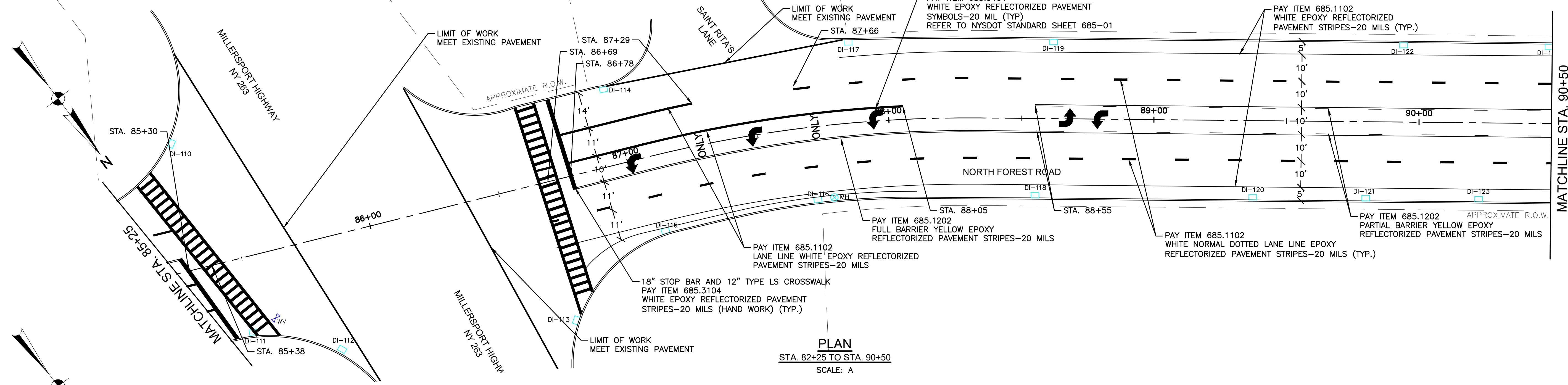
V:\Public Works\Highways - Main\Road Files\Overlays\2026 Overlay Program\NorthForest

MATCHLINE STA. 79+75
 DRAWING NFP-5

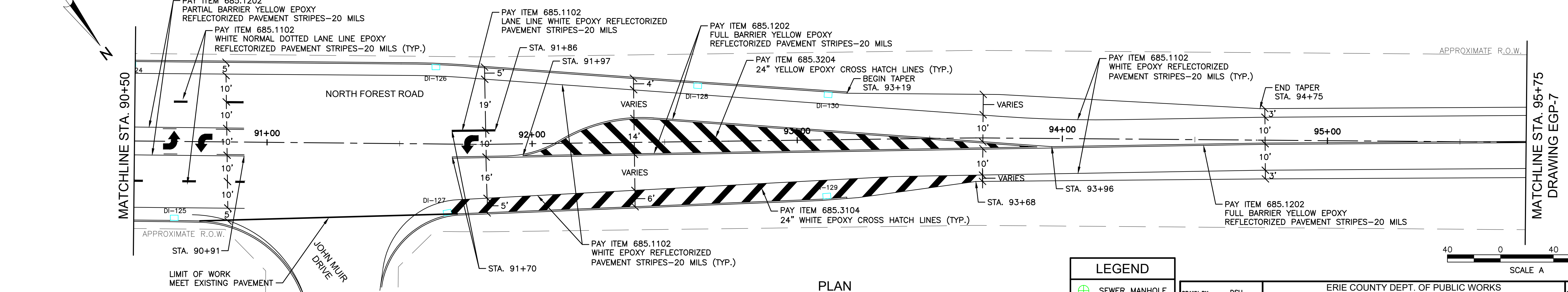
MATCHLINE STA. 85+25
 DRAWING EGP-7



PLAN
 STA. 79+75 TO STA. 85+25
 SCALE: A



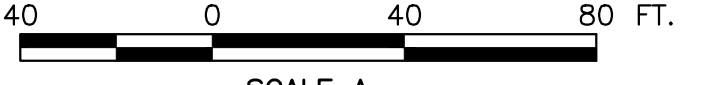
PLAN
 STA. 82+25 TO STA. 90+50
 SCALE: A

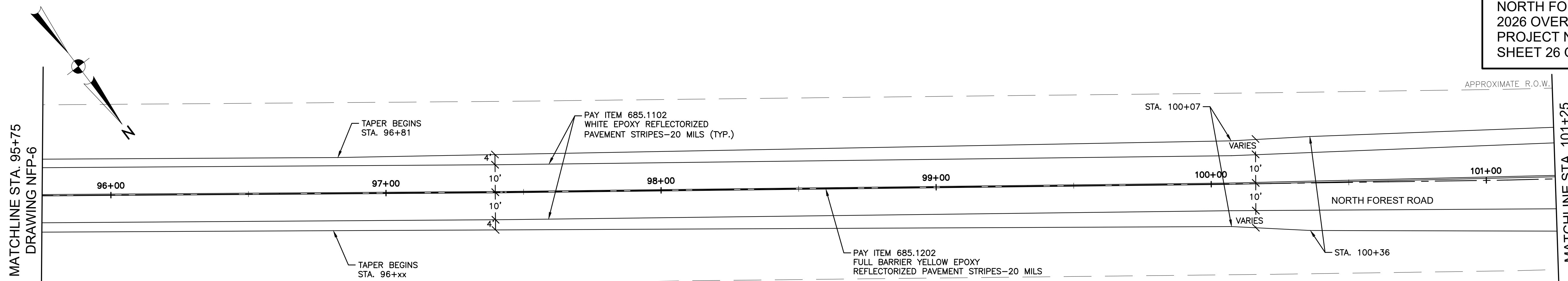


PLAN
 STA. 90+50 TO STA. 95+75
 SCALE: A

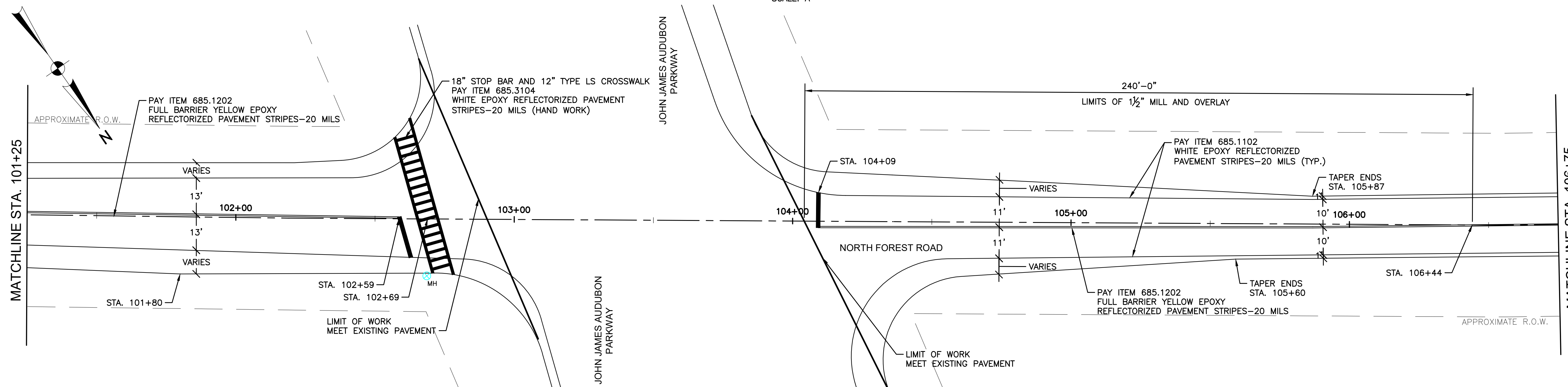
| LEGEND | |
|--------|----------------|
| | SEWER MANHOLE |
| | WATER VALVE |
| | DRAINAGE INLET |

| | | |
|-------------------------|-----------------------------------|--------------------|
| DRAWN BY: DSJ | ERIE COUNTY DEPT. OF PUBLIC WORKS | SHEET NO. 25 OF 41 |
| CHECKED BY: GMW | DIVISION OF HIGHWAYS | DATE: MAY 26 |
| APPROVED BY: DMS | 95 FRANKLIN ST. BUFFALO, N.Y. | SCALE: 1" = 20' |
| DATE COMPLETED: 5/12/26 | NORTH FOREST ROAD - CR 294 | NFP-6 |
| | MILL AND OVERLAY PLANS (6 OF 12) | |

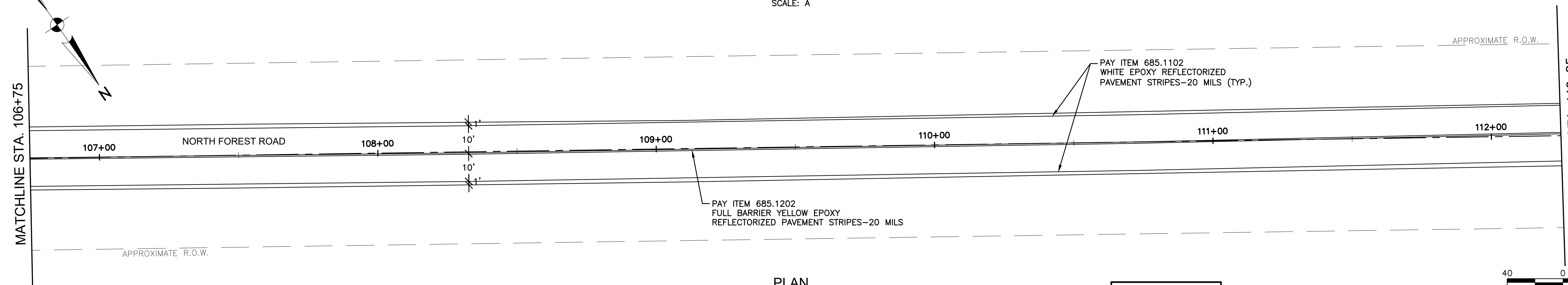




PLAN
 STA. 95+75 TO STA. 101+25
 SCALE: A



PLAN
 STA. 101+25 TO STA. 106+75
 SCALE: A



PLAN
 STA. 106+75 TO STA. 112+25
 SCALE: A

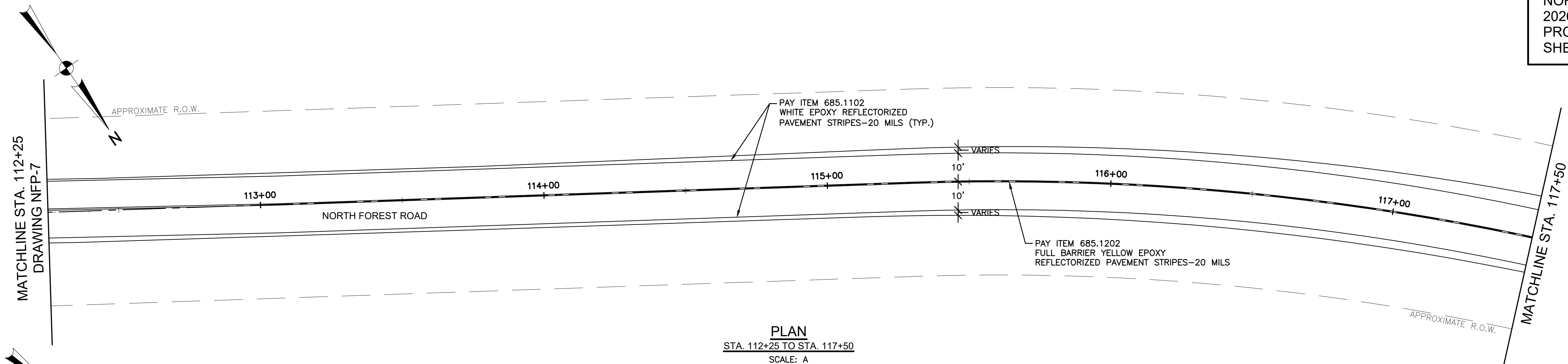
| LEGEND | |
|--------|----------------|
| | SEWER MANHOLE |
| | WATER VALVE |
| | DRAINAGE INLET |

| | |
|----------------|---------|
| DRAWN BY | DSU |
| CHECKED BY | GMW |
| APPROVED BY | DMS |
| DATE COMPLETED | 5/13/26 |

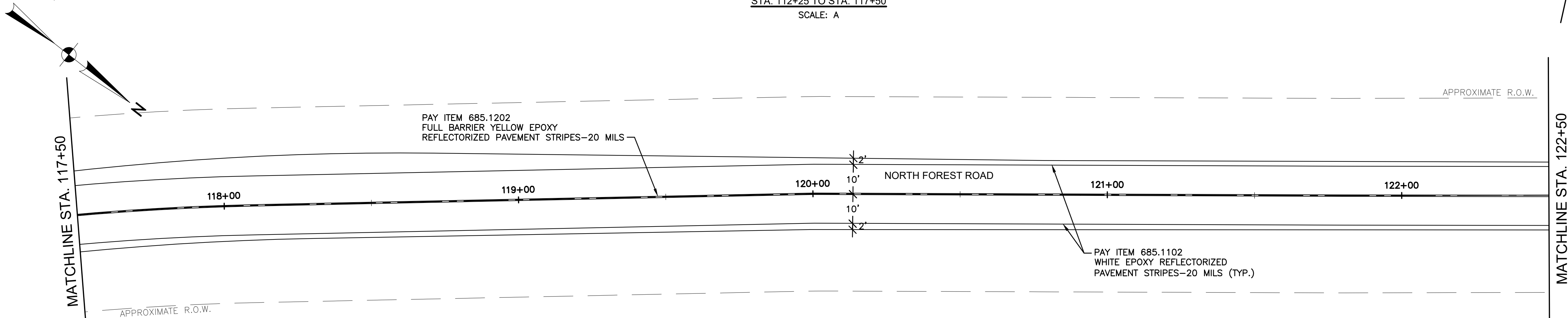
| | |
|-----------------------------------|---------------|
| ERIE COUNTY DEPT. OF PUBLIC WORKS | |
| DIVISION OF HIGHWAYS | |
| 95 FRANKLIN ST | BUFFALO, N.Y. |
| NORTH FOREST ROAD - CR 294 | |
| MILL AND OVERLAY PLANS (7 OF 12) | |

| | |
|-----------|----------|
| SHEET NO. | 26 OF 41 |
| DATE | MAY 26 |
| SCALE | 1" = 20' |
| | NFP-7 |

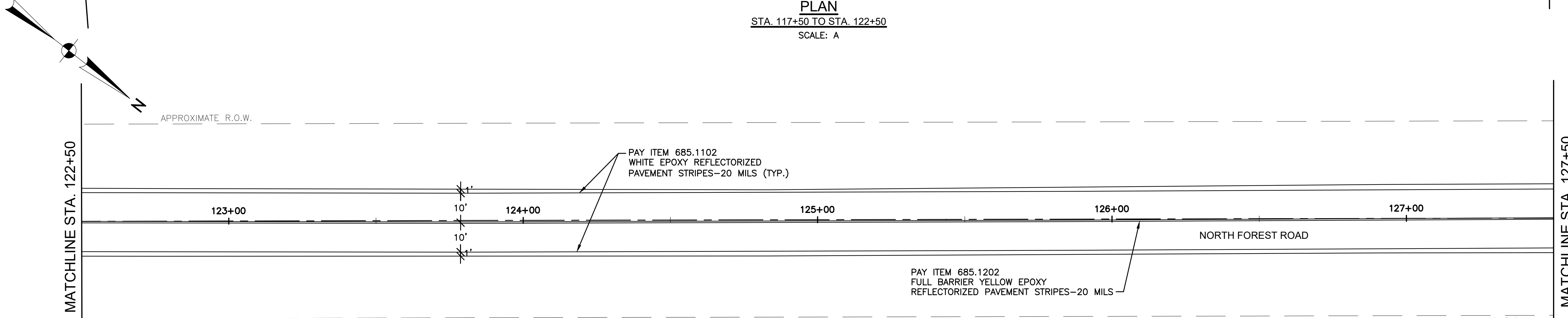




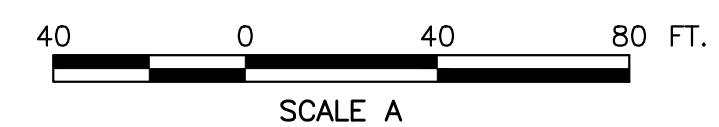
PLAN
 STA. 112+25 TO STA. 117+50
 SCALE: A



PLAN
 STA. 117+50 TO STA. 122+50
 SCALE: A



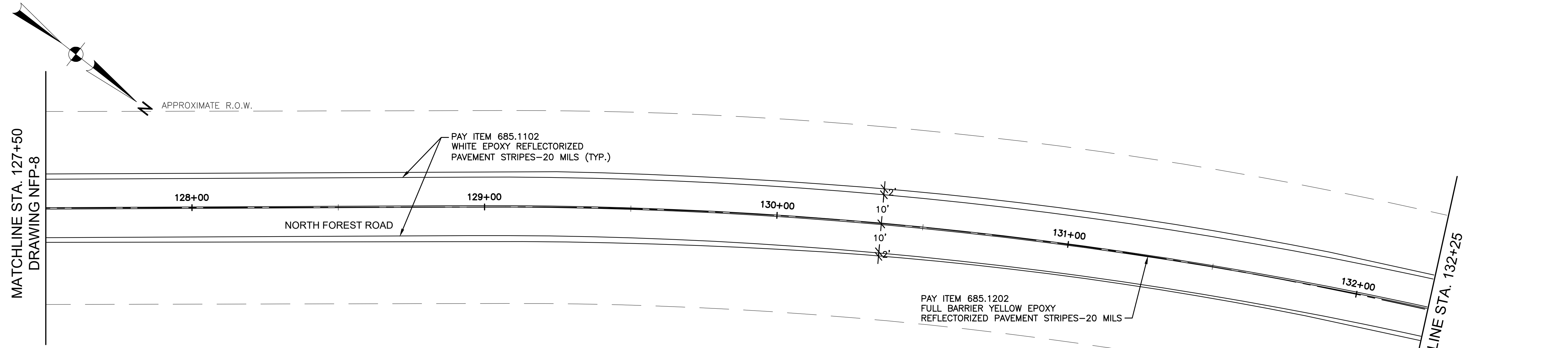
PLAN
 STA. 122+50 TO STA. 127+50
 SCALE: A



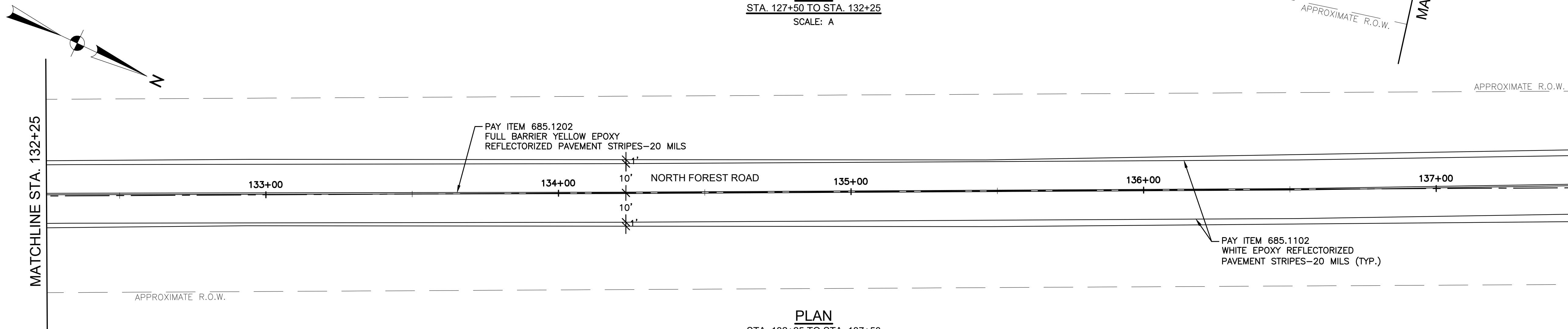
| LEGEND | |
|--------|----------------|
| | SEWER MANHOLE |
| | WATER VALVE |
| | DRAINAGE INLET |

| | | |
|-------------------------|--|--------------------|
| DRAWN BY: DSJ | ERIE COUNTY DEPT. OF PUBLIC WORKS DIVISION OF HIGHWAYS 95 FRANKLIN ST. BUFFALO, N.Y. | SHEET NO. 27 OF 41 |
| CHECKED BY: GMW | | DATE: MAY 26 |
| APPROVED BY: DMS | NORTH FOREST ROAD - CR 294 MILL AND OVERLAY PLANS (8 OF 12) | SCALE: 1" = 20' |
| DATE COMPLETED: 5/12/26 | | NFP-8 |

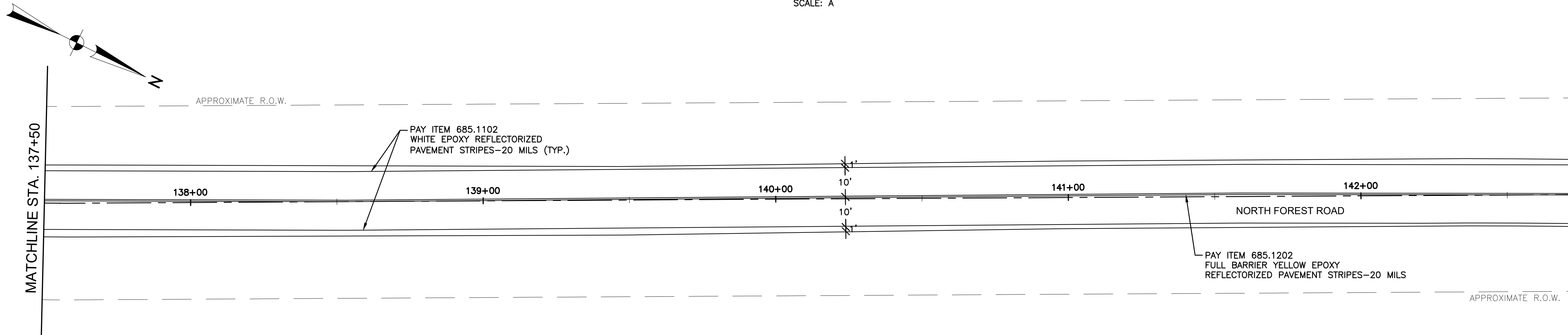
V:\Public Works\Highways - Main\Road Files\Overlays\2026 Overlay Program\NorthForest



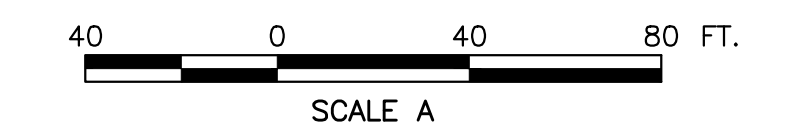
PLAN
 STA. 127+50 TO STA. 132+25
 SCALE: A



PLAN
 STA. 132+25 TO STA. 137+50
 SCALE: A



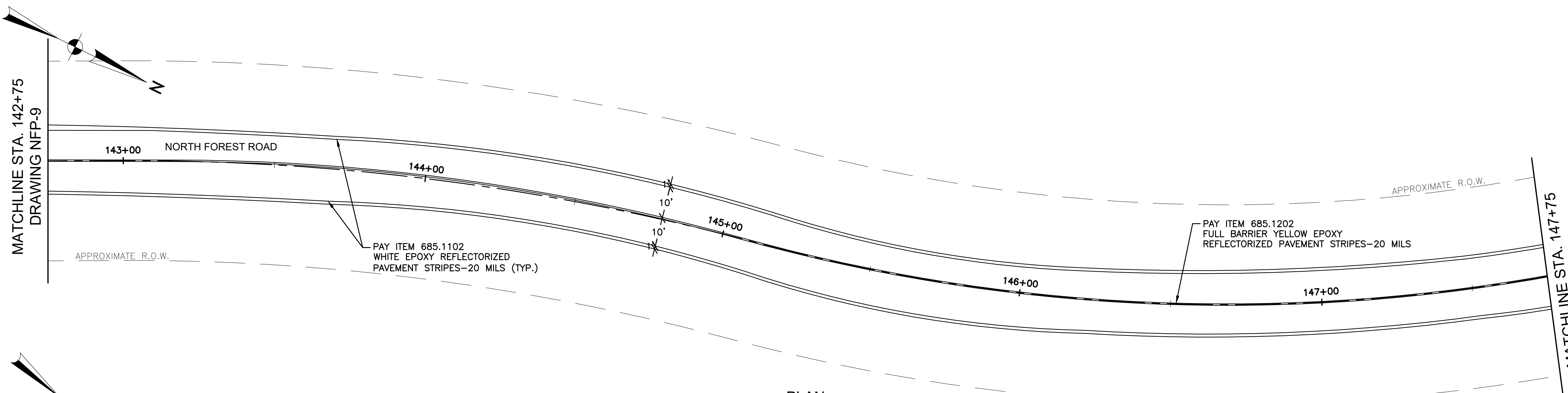
PLAN
 STA. 137+50 TO STA. 142+75
 SCALE: A



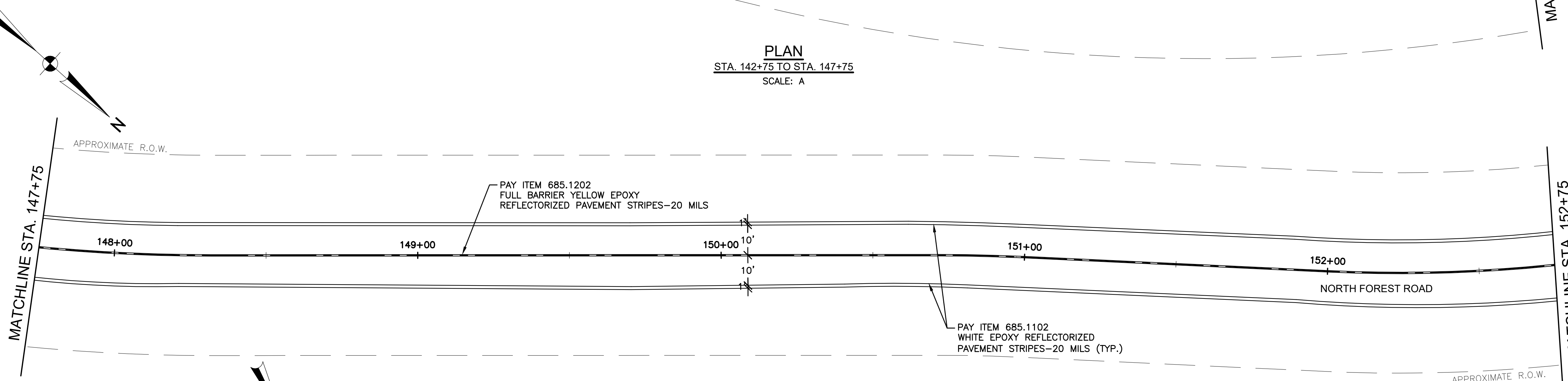
| LEGEND | |
|--------|----------------|
| | SEWER MANHOLE |
| | WATER VALVE |
| | DRAINAGE INLET |

| | | |
|-------------------------|--|--------------------|
| DRAWN BY: DSJ | ERIE COUNTY DEPT. OF PUBLIC WORKS DIVISION OF HIGHWAYS 95 FRANKLIN ST. BUFFALO, N.Y. | SHEET NO. 28 OF 41 |
| CHECKED BY: GMW | | DATE: MAY 26 |
| APPROVED BY: DMS | NORTH FOREST ROAD - CR 294 MILL AND OVERLAY PLANS (9 OF 12) | SCALE: 1" = 20' |
| DATE COMPLETED: 5/12/26 | | NFP-9 |

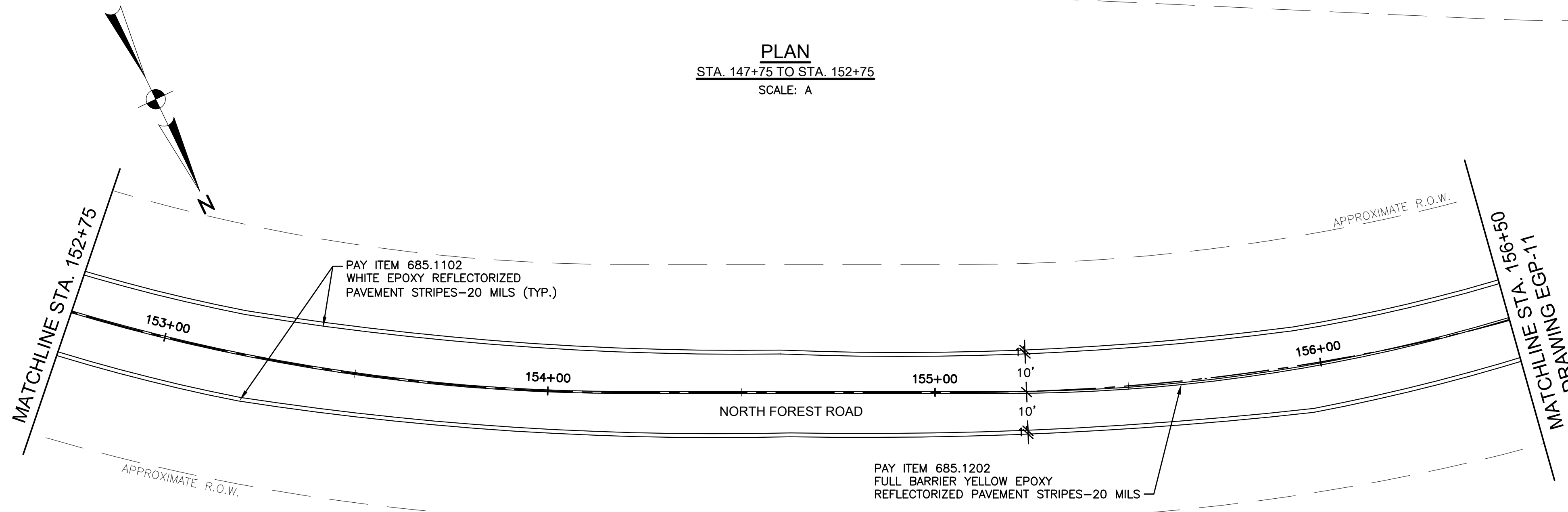
V:\Public Works\Highways - Main\Road Files\Overlays\2026 Overlay Program\NorthForest



PLAN
 STA. 142+75 TO STA. 147+75
 SCALE: A



PLAN
 STA. 147+75 TO STA. 152+75
 SCALE: A

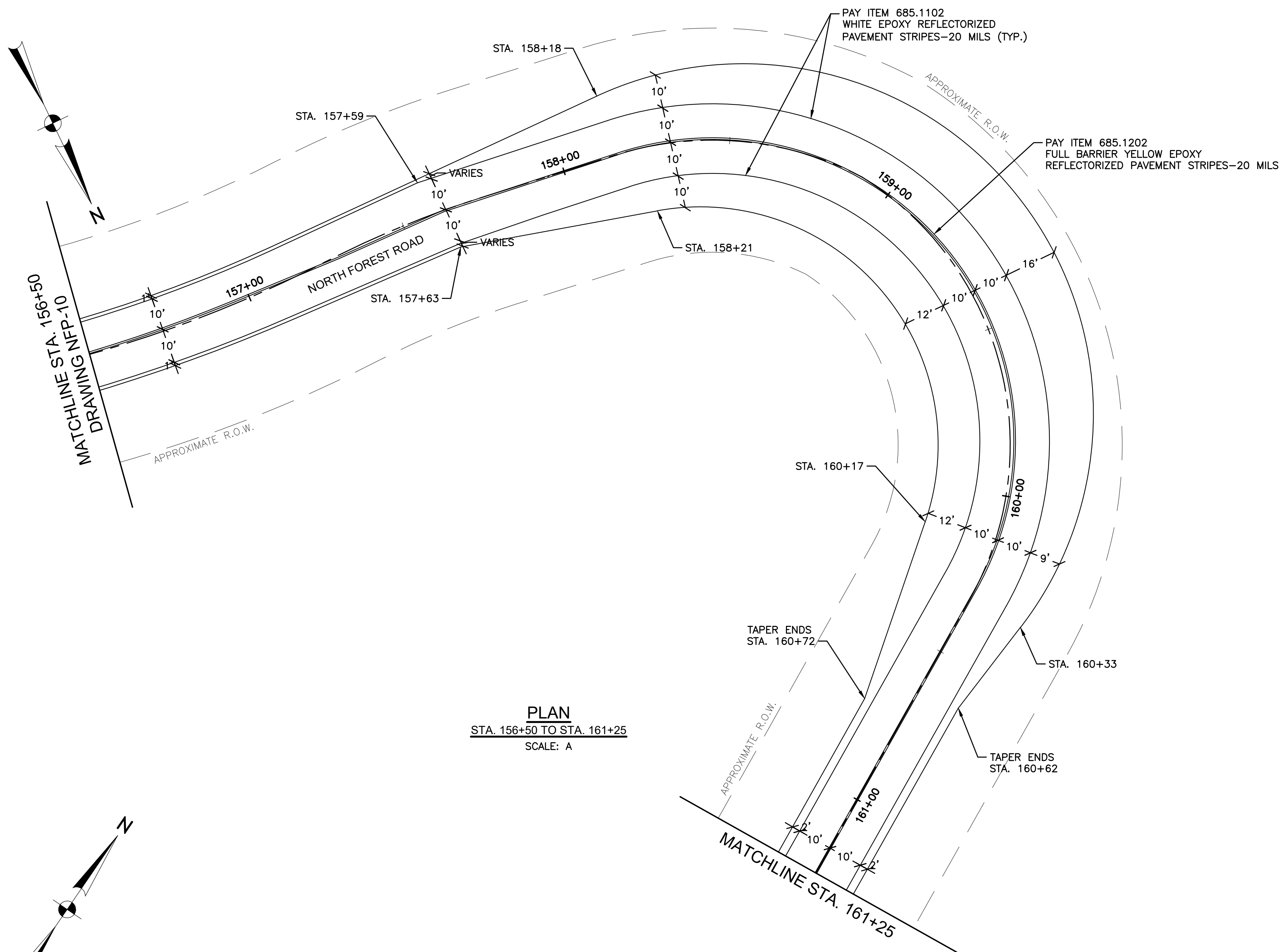


PLAN
 STA. 152+75 TO STA. 156+50
 SCALE: A

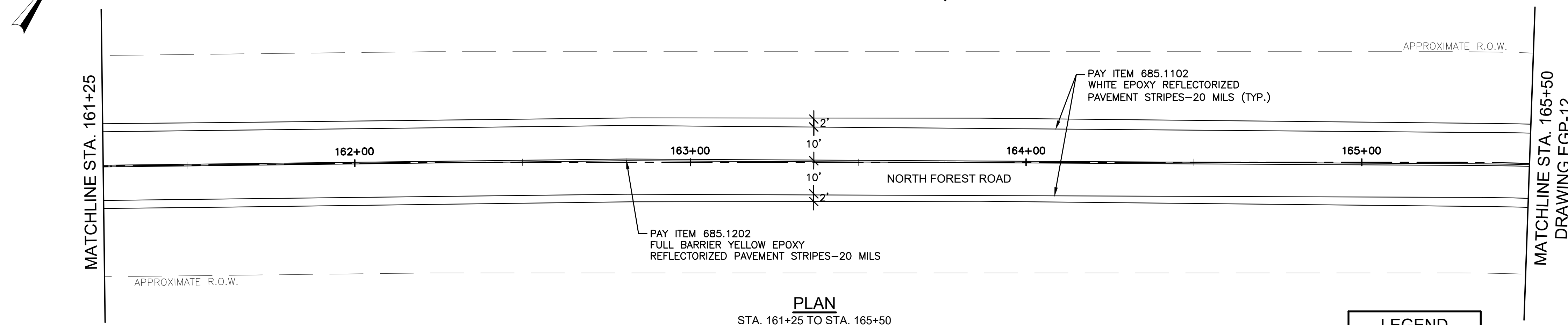
| LEGEND | |
|--------|----------------|
| | SEWER MANHOLE |
| | WATER VALVE |
| | DRAINAGE INLET |



| | | |
|-------------------------|---|--------------------|
| DRAWN BY: DSJ | ERIE COUNTY DEPT. OF PUBLIC WORKS | SHEET NO. 29 OF 41 |
| CHECKED BY: GMW | DIVISION OF HIGHWAYS | DATE: MAY 26 |
| APPROVED BY: DMS | 95 FRANKLIN ST. BUFFALO, N.Y. | SCALE: 1" = 20' |
| DATE COMPLETED: 5/12/26 | NORTH FOREST ROAD - CR 294 MILL AND OVERLAY PLANS (10 OF 12) | NFP-10 |

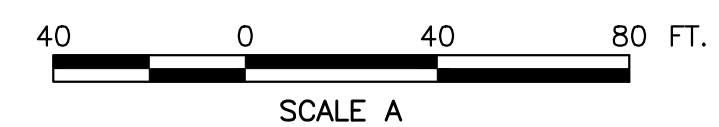


PLAN
 STA. 156+50 TO STA. 161+25
 SCALE: A



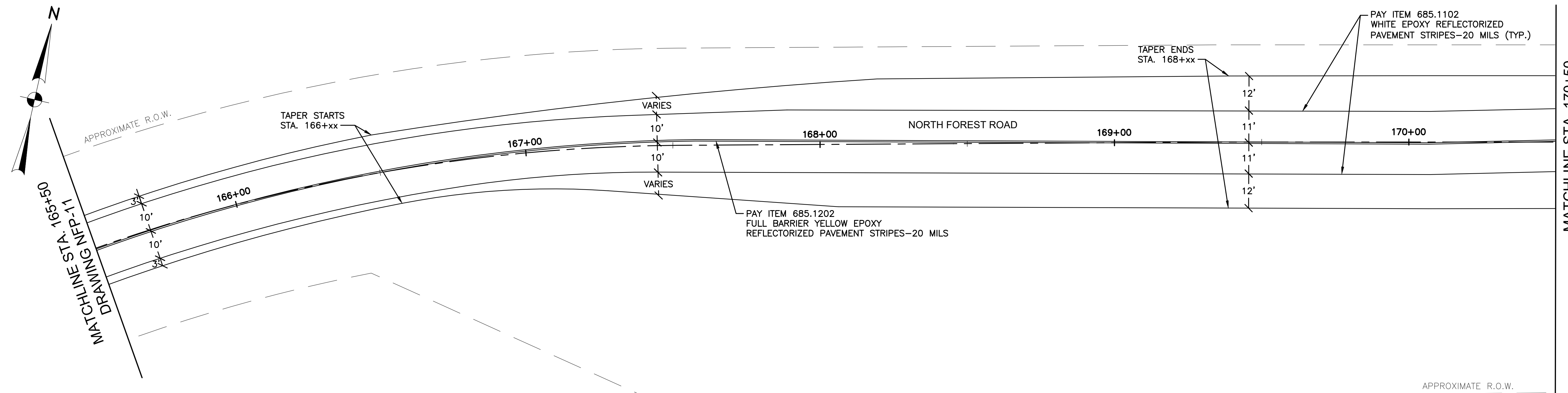
PLAN
 STA. 161+25 TO STA. 165+50
 SCALE: A

| LEGEND | |
|--------|----------------|
| | SEWER MANHOLE |
| | WATER VALVE |
| | DRAINAGE INLET |

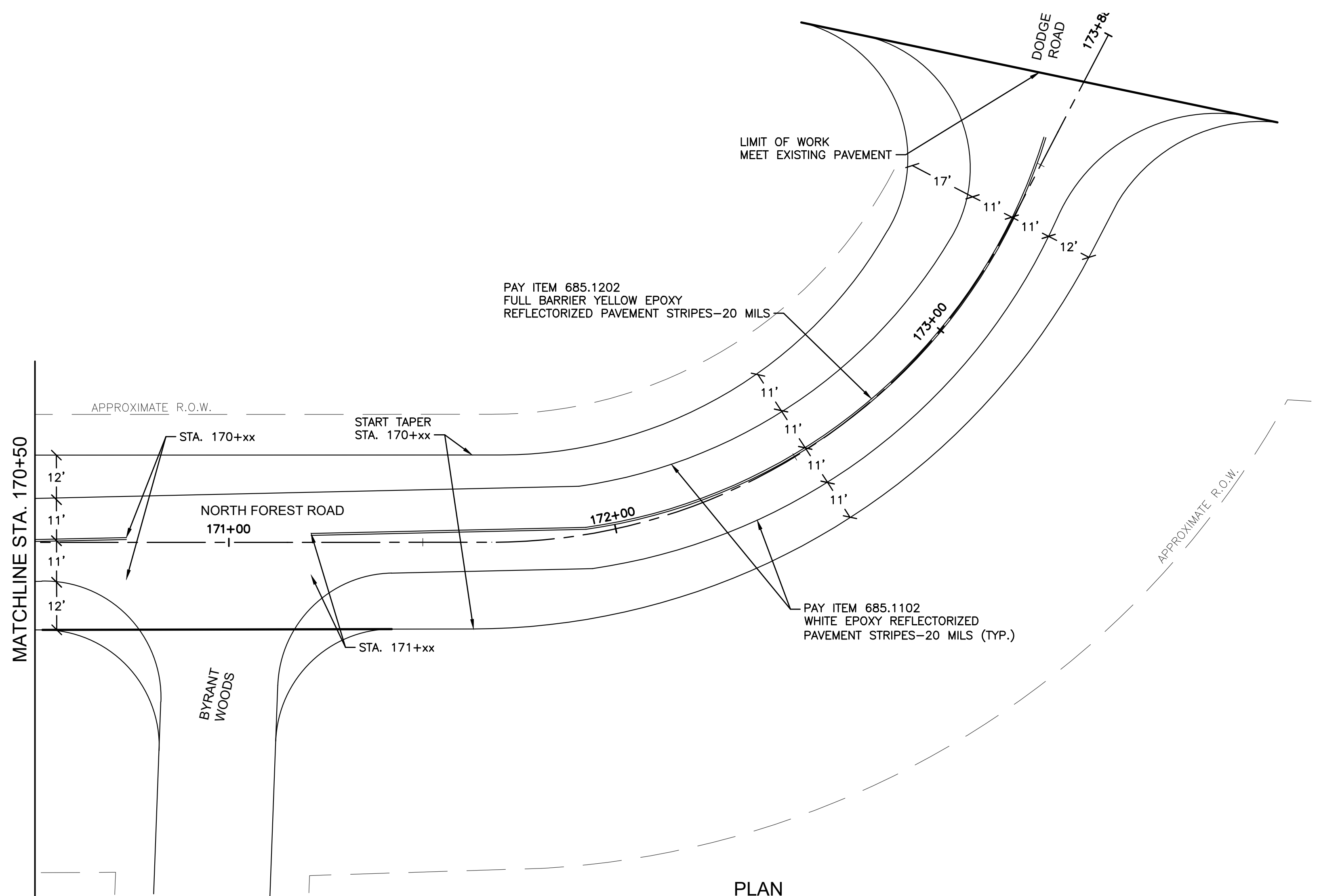


| | | |
|-------------------------|---|--------------------|
| DRAWN BY: DSJ | ERIE COUNTY DEPT. OF PUBLIC WORKS | SHEET NO. 30 OF 41 |
| CHECKED BY: GMW | DIVISION OF HIGHWAYS | DATE: MAY 26 |
| APPROVED BY: DMS | 95 FRANKLIN ST. BUFFALO, N.Y. | SCALE: 1" = 20' |
| DATE COMPLETED: 5/12/26 | NORTH FOREST ROAD - CR 294 MILL AND OVERLAY PLANS (11 OF 12) | NFP-11 |

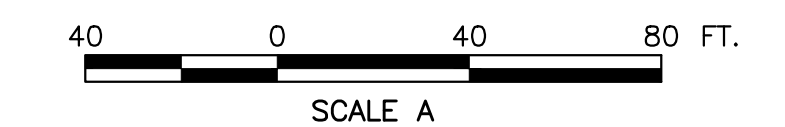
V:\Public Works\Highways - Main\Road Files\Overlays\2026 Overlay Program\NorthForest



PLAN
 STA. 165+50 TO STA. 170+50
 SCALE: A



PLAN
 STA. 170+50 TO STA. 173+88
 SCALE: A



| LEGEND | |
|--------|----------------|
| | SEWER MANHOLE |
| | WATER VALVE |
| | DRAINAGE INLET |

| | | |
|-------------------------|---|--------------------|
| DRAWN BY: DSJ | ERIE COUNTY DEPT. OF PUBLIC WORKS | SHEET NO. 31 OF 41 |
| CHECKED BY: GMW | DIVISION OF HIGHWAYS | DATE: MAY 26 |
| APPROVED BY: DMS | 95 FRANKLIN ST BUFFALO, N.Y. | SCALE: 1" = 20' |
| DATE COMPLETED: 5/12/26 | NORTH FOREST ROAD - CR 294 MILL AND OVERLAY PLANS (12 OF 12) | NFP-12 |

V:\Public Works\Highways - Main\Road Files\Overlays\2026 Overlay Program\NorthForest

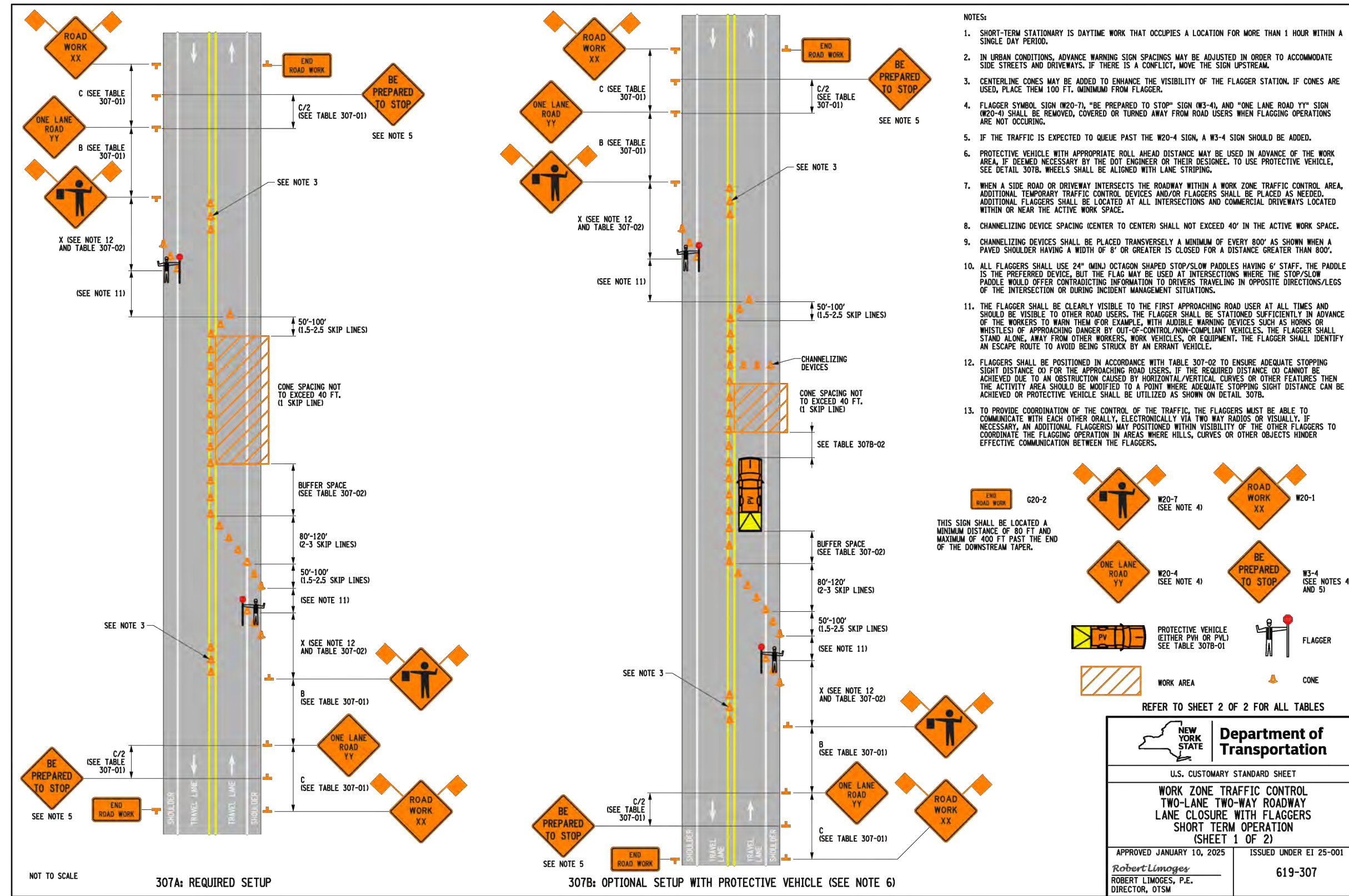


TABLE 307-01: ADVANCE WARNING SIGN SPACING

| ROAD TYPE & SPEED | DISTANCE BETWEEN SIGNS | SD SIGN LEGEND |
|-------------------|------------------------|----------------|
| URBAN < 30 MPH | 100 150 200 | 100' 150' 200' |
| URBAN 30-40 MPH | 200 300 400 | 200' 300' 400' |
| URBAN 40-45 MPH | 300 400 500 | 300' 400' 500' |
| RURAL | 500 500 500 | 500' 500' 500' |

* PRECONSTRUCTION POSTED SPEED LIMIT

TABLE 307-02: LONGITUDINAL BUFFER SPACE / STOPPING SIGHT DISTANCE (SD)

| PRECONSTRUCTION POSTED SPEED LIMIT (MPH) | LONGITUDINAL BUFFER SPACE DISTANCE (FT) / # OF SKIP LINES |
|--|---|
| 25 | 150/4 |
| 30 | 200/5 |
| 35 | 250/6 |
| 40 | 300/8 |
| 45 | 360/9 |
| 50 | 425/11 |
| 55 | 495/13 |

TABLE 307-03: REQUIRED SIGN SIZES*

| SD SIGN | NON-FREEMAY | FREEMAY |
|---------|-------------|---------|
| W20-2 | 36x18 | 48x24 |
| W2-4 | 36x26 | 48x48 |
| W20-1 | 36x26 | 48x48 |
| W20-4 | 36x26 | 48x48 |
| W20-7 | 36x26 | 48x48 |
| W20-8 | 18x18 | 18x18 |

* FREEMAY SIGNS MAY BE USED ON NON-FREEMAY, IF SPACE CONSTRAINTS DO NOT EXIST.

TABLE 307B-01: PROTECTIVE VEHICLE REQUIREMENTS

| CLOSURE TYPE | ROAD TYPE & SPEED | NON-FREEMAY | | |
|----------------------------------|---|-------------|-------------|------------|
| | | ≥ 45 MPH | 35 - 40 MPH | ≤ 30 MPH |
| LANE CLOSURE OR ENCHANCEMENT | EXPOSURE CONDITIONS (SEE NOTE 1) | PNH+TMA | PL+TMA | PL |
| | WORKERS ON FOOT OR WORK VEHICLE EXPOSED TO TRAFFIC | PNH+TMA | PL | SEE NOTE 2 |
| SHOULDER CLOSURE OR ENCHANCEMENT | WORKERS ON FOOT OR WORK VEHICLE EXPOSED TO TRAFFIC | PNH+TMA | PL | PL |
| | NO WORKERS ON FOOT OR WORK VEHICLE EXPOSED TO TRAFFIC | PNH+TMA | PL | SEE NOTE 2 |

LEGEND:
 PNL - PROTECTIVE VEHICLE LIGHT MINIMUM GROSS WEIGHT 9,500 LBS. OR GREATER (SEE NOTE 4)
 PNH - PROTECTIVE VEHICLE HEAVY MINIMUM GROSS WEIGHT 22,000 LBS. OR GREATER
 TMA - TRUCK/TRAILER MOUNTED IMPACT ATTENUATOR

NOTES:
 1. THE EXPOSURE CONDITIONS ASSUME THERE IS NO POSITIVE PROTECTION PRESENT.
 2. EITHER A PROTECTIVE LIGHT (PNL) OR THE STANDARD BUFFER SPACE (SEE TABLE 307-02) SHALL BE PROVIDED.
 3. TRUCK/TRAILER MOUNTED IMPACT ATTENUATORS (TMA) SHALL NOT BE MOUNTED/INSTALLED ON VEHICLES WITH A GROSS VEHICLE WEIGHT (GVW) LESS THAN WHAT IS MINIMALLY REQUIRED BY THE MANUFACTURER OF THE TMA.
 4. THE USE OF A PROTECTIVE VEHICLE LIGHT (PNL) AS A SHADOW VEHICLE IS LIMITED TO NON-FREEMAY ROADWAYS WHERE THE POSTED SPEED LIMITS IS ≤ 40 MPH UNLESS OTHERWISE AUTHORIZED BY THE ENGINEER.

TABLE 307B-02: ROLL-AHEAD DISTANCE FOR PROTECTIVE VEHICLES

| PRECONSTRUCTION POSTED SPEED LIMIT (MPH) | STATIONARY OPERATION | |
|--|---|---|
| | PROTECTIVE VEHICLES WEIGHING 9,500 TO 21,999 LBS. GVW | PROTECTIVE VEHICLES WEIGHING 22,000 LBS. OR GREATER GVW |
| 45 - 55 | 160/4 | 120/3 |
| ≤ 40 | 120/3 | 80/2 |

NEW YORK DEPARTMENT OF TRANSPORTATION
U.S. CUSTOMARY STANDARD SHEET
WORK ZONE TRAFFIC CONTROL
TWO-LANE TWO-WAY ROADWAY
LANE CLOSURE WITH FLAGGERS
SHORT TERM OPERATION
(SHEET 2 OF 2)
APPROVED JANUARY 10, 2025 ISSUED UNDER ET 25-001
Robert Linsag, P.E. DIRECTOR, OTSM 619-307

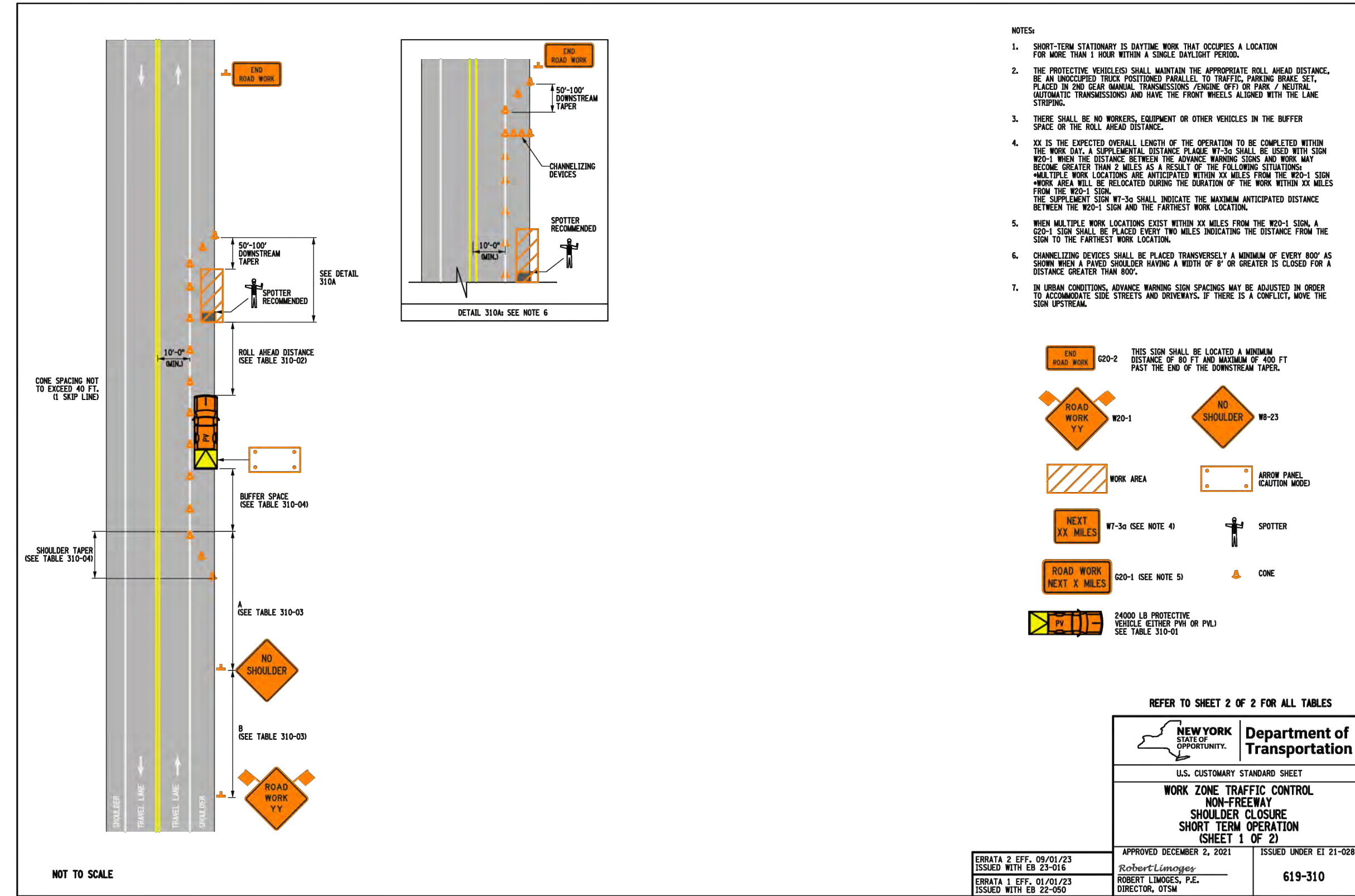


TABLE 310-01: PROTECTIVE VEHICLE REQUIREMENTS

| CLOSURE TYPE | ROAD TYPE & SPEED | NON-FREEMAY | | |
|----------------------------------|---|-------------|-------------|------------|
| | | ≥ 45 MPH | 35 - 40 MPH | ≤ 30 MPH |
| LANE CLOSURE OR ENCHANCEMENT | EXPOSURE CONDITIONS (SEE NOTE 1) | PNH+TMA | PL+TMA | PL |
| | WORKERS ON FOOT OR WORK VEHICLE EXPOSED TO TRAFFIC | PNH+TMA | PL | SEE NOTE 2 |
| SHOULDER CLOSURE OR ENCHANCEMENT | WORKERS ON FOOT OR WORK VEHICLE EXPOSED TO TRAFFIC | PNH+TMA | PL | PL |
| | NO WORKERS ON FOOT OR WORK VEHICLE EXPOSED TO TRAFFIC | PNH+TMA | PL | SEE NOTE 2 |

LEGEND:
 PNL - PROTECTIVE VEHICLE LIGHT MINIMUM GROSS WEIGHT 9,500 LBS. OR GREATER (SEE NOTE 4)
 PNH - PROTECTIVE VEHICLE HEAVY MINIMUM GROSS WEIGHT 22,000 LBS. OR GREATER
 TMA - TRUCK/TRAILER MOUNTED IMPACT ATTENUATOR

NOTES:
 1. THE EXPOSURE CONDITIONS ASSUME THERE IS NO POSITIVE PROTECTION PRESENT.
 2. EITHER A PROTECTIVE LIGHT (PNL) OR THE STANDARD BUFFER SPACE (SEE TABLE 310-03) SHALL BE PROVIDED.
 3. TRUCK/TRAILER MOUNTED IMPACT ATTENUATORS (TMA) SHALL NOT BE MOUNTED/INSTALLED ON VEHICLES WITH A GROSS VEHICLE WEIGHT (GVW) LESS THAN WHAT IS MINIMALLY REQUIRED BY THE MANUFACTURER OF THE TMA.
 4. THE USE OF A PROTECTIVE VEHICLE LIGHT (PNL) AS A SHADOW VEHICLE IS LIMITED TO NON-FREEMAY ROADWAYS WHERE THE POSTED SPEED LIMITS IS ≤ 40 MPH UNLESS OTHERWISE AUTHORIZED BY THE ENGINEER.

TABLE 310-02: ROLL-AHEAD DISTANCE FOR PROTECTIVE VEHICLES

| PRECONSTRUCTION POSTED SPEED LIMIT (MPH) | STATIONARY OPERATION | |
|--|---|---|
| | PROTECTIVE VEHICLES WEIGHING 9,500 TO 21,999 LBS. GVW | PROTECTIVE VEHICLES WEIGHING 22,000 LBS. OR GREATER GVW |
| 45 - 55 | 160/4 | 120/3 |
| ≤ 40 | 120/3 | 80/2 |

TABLE 310-03: ADVANCE WARNING SIGN SPACING

| ROAD TYPE & SPEED | DISTANCE BETWEEN SIGNS | SD SIGN LEGEND |
|-------------------|------------------------|----------------|
| URBAN < 30 MPH | 100 150 200 | 100' 150' 200' |
| URBAN 30-40 MPH | 200 300 400 | 200' 300' 400' |
| URBAN 40-45 MPH | 300 400 500 | 300' 400' 500' |
| RURAL | 500 500 500 | 500' 500' 500' |

* PRECONSTRUCTION POSTED SPEED LIMIT

TABLE 310-04: LONGITUDINAL BUFFER SPACE AND TAPER LENGTHS

| PRECONSTRUCTION POSTED SPEED LIMIT (MPH) | LONGITUDINAL BUFFER SPACE DISTANCE (FT) / # OF SKIP LINES | TAPER LENGTH (FT) / # OF SKIP LINES / # OF CHANNELIZING DEVICES | | | | SHOULDER TAPER LENGTH (FT) / # OF SKIP LINES / # OF CHANNELIZING DEVICES | | | | | |
|--|---|---|-----------|-----------|---------|--|---------|---------|---------|---------|---------|
| | | 10 | 11 | 12 | ≤ 4 FT. | 5 - 7 FT. | 8 FT. | 9 FT. | 10 FT. | 11 FT. | 12 FT. |
| 25 | 155 / 4 | 120/2/4 | 120/2/4 | 120/2/4 | 40/1/2 | 40/1/2 | 40/1/2 | 40/1/2 | 40/1/2 | 40/1/2 | 40/1/2 |
| 30 | 200 / 5 | 160/4/5 | 160/4/5 | 200/5/6 | 40/1/2 | 40/1/2 | 40/1/2 | 40/1/2 | 40/1/2 | 40/1/2 | 40/1/2 |
| 35 | 250 / 6 | 200/5/6 | 240/6/7 | 240/6/7 | 40/1/2 | 40/1/2 | 40/1/2 | 40/1/2 | 40/1/2 | 40/1/2 | 40/1/2 |
| 40 | 305 / 8 | 280/7/8 | 320/8/9 | 320/8/9 | 40/1/2 | 40/1/2 | 40/1/2 | 40/1/2 | 40/1/2 | 40/1/2 | 40/1/2 |
| 45 | 360 / 9 | 440/11/12 | 520/13/14 | 560/14/15 | 80/2/3 | 80/2/3 | 120/3/4 | 120/3/4 | 120/3/4 | 120/3/4 | 160/4/5 |
| 50 | 425 / 11 | 520/13/14 | 560/14/15 | 600/15/16 | 80/2/3 | 120/3/4 | 160/4/5 | 160/4/5 | 160/4/5 | 160/4/5 | 200/5/6 |
| 55 | 495 / 13 | 560/14/15 | 600/15/16 | 680/17/18 | 80/2/3 | 120/3/4 | 160/4/5 | 160/4/5 | 160/4/5 | 200/5/6 | 200/5/6 |

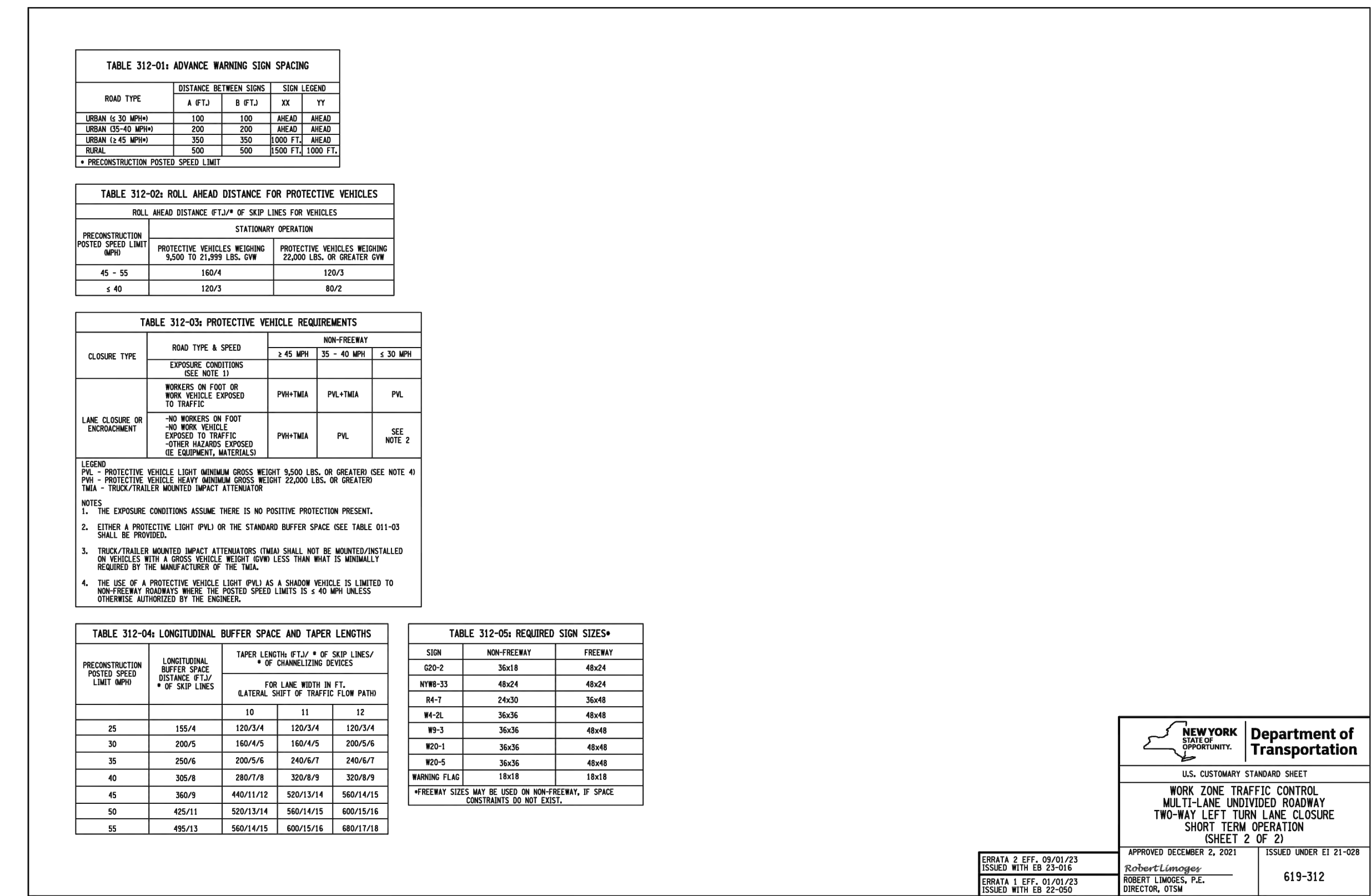
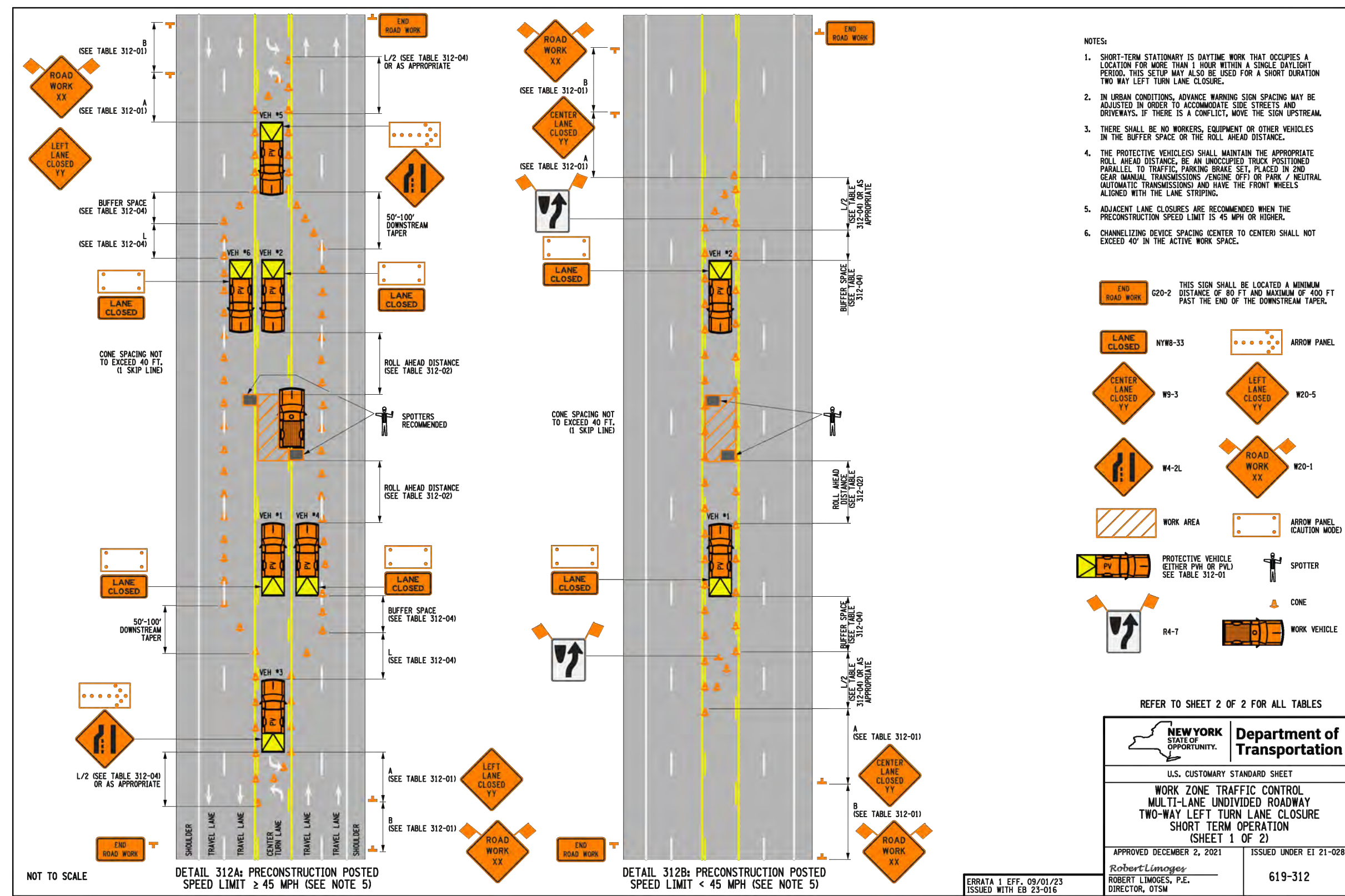
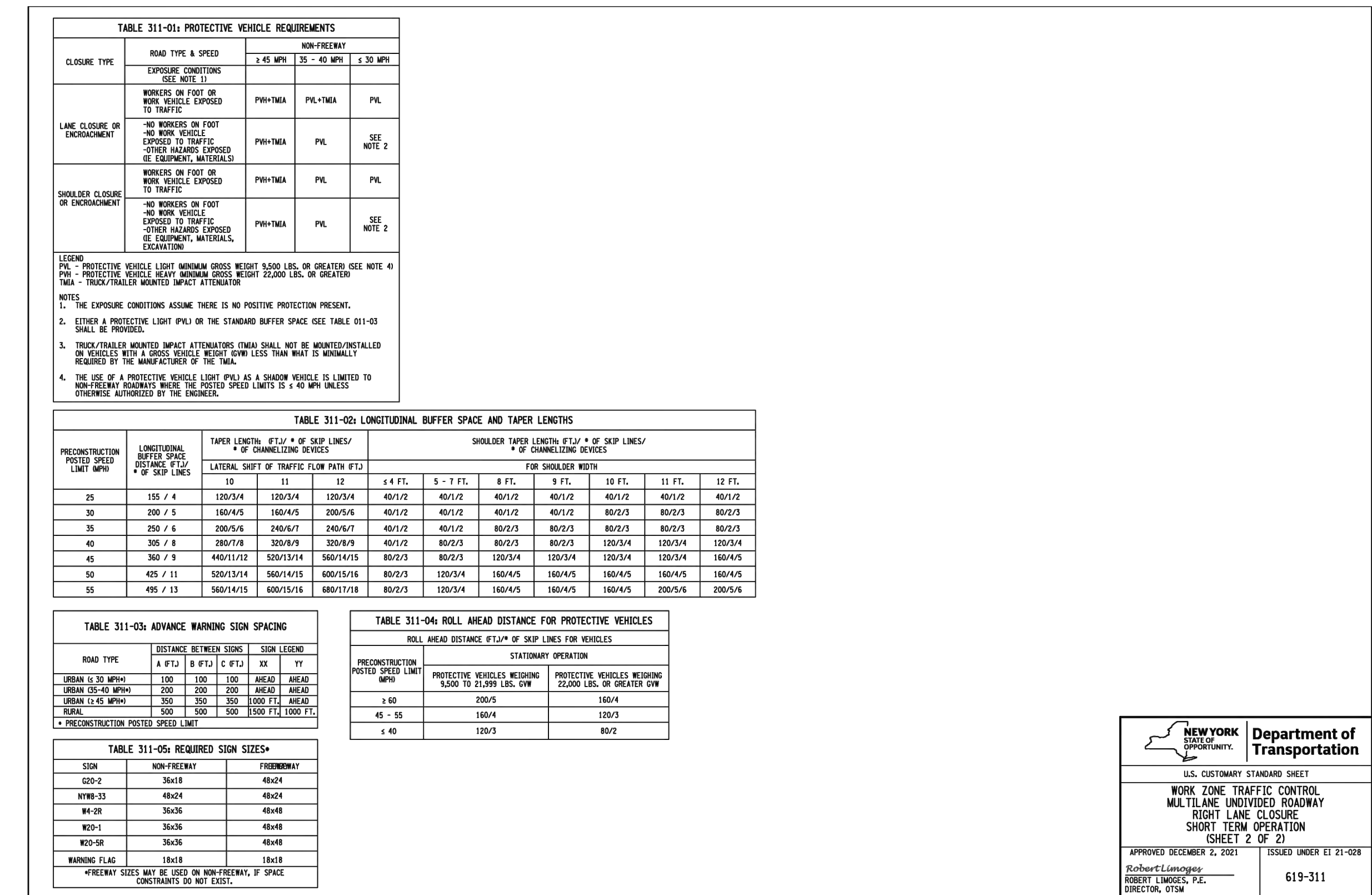
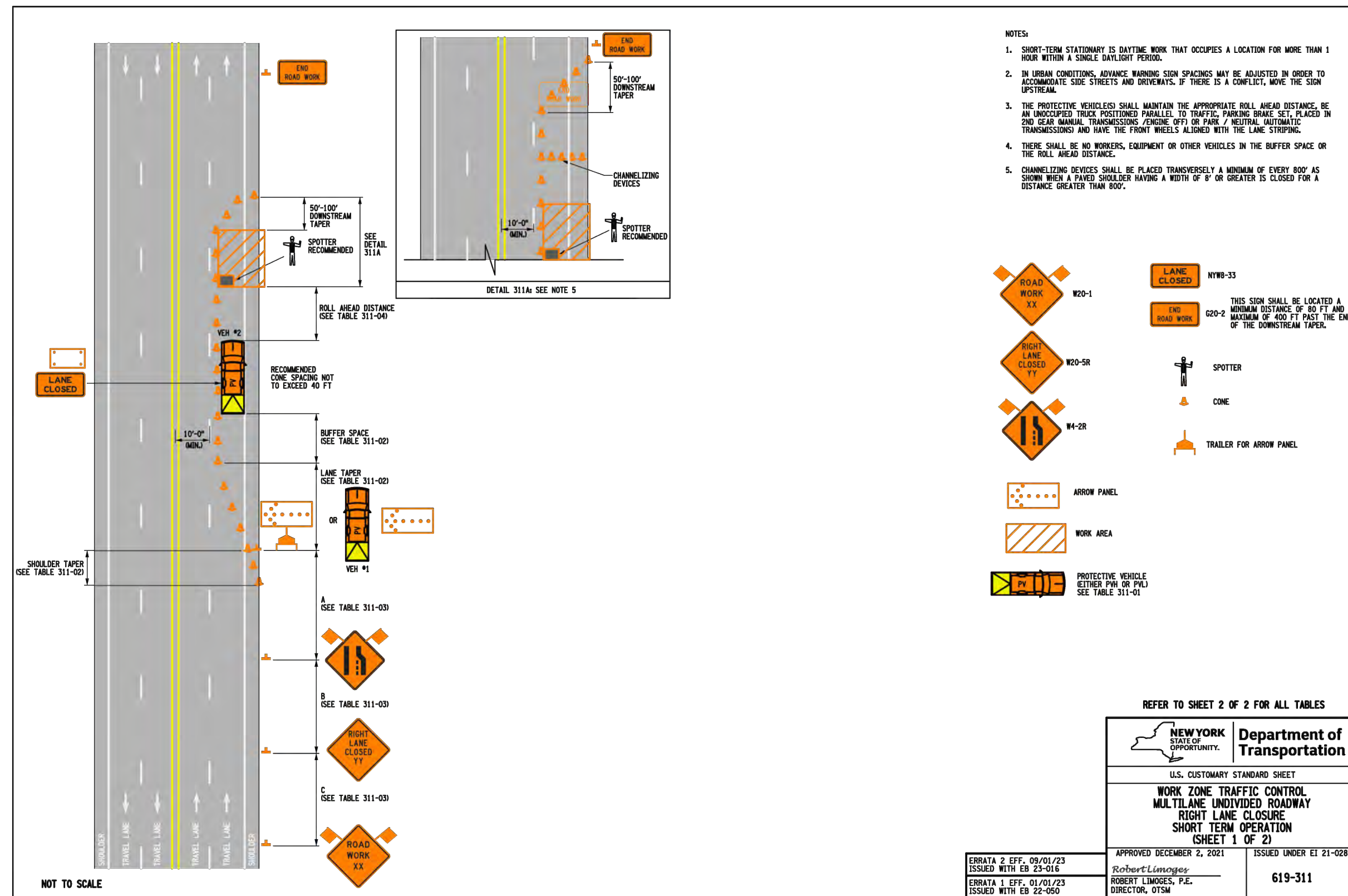
TABLE 310-05: REQUIRED SIGN SIZES*

| SD SIGN | NON-FREEMAY | FREEMAY |
|---------|-------------|---------|
| W20-2 | 36x18 | 48x24 |
| W20-3 | 36x18 | 48x24 |
| W20-4 | 36x26 | 48x48 |
| W20-5 | 36x26 | 48x48 |
| W20-6 | 36x26 | 48x48 |
| W20-7 | 18x18 | 18x18 |

* FREEMAY SIGNS MAY BE USED ON NON-FREEMAY, IF SPACE CONSTRAINTS DO NOT EXIST.

NEW YORK DEPARTMENT OF TRANSPORTATION
U.S. CUSTOMARY STANDARD SHEET
WORK ZONE TRAFFIC CONTROL
NON-FREEMAY
SHOULDER CLOSURE
SHORT TERM OPERATION
(SHEET 2 OF 2)
APPROVED DECEMBER 2, 2021 ISSUED UNDER ET 21-028
Robert Linsag, P.E. DIRECTOR, OTSM 619-310

v:\Public Works\Highways - Main\Road Files\Overlays\2026 Overlay Program\2026WZTC.dwg



| ROAD TYPE | A | B | C | D | E | F | G | H | I | J | K | L | M | N | O | P | Q | R | S | T | U | V | W | X | Y | Z |
|-----------------|-----|-----|-----|-----|-----|-----|-----|-----|-----|-----|-----|-----|-----|-----|-----|-----|-----|-----|-----|-----|-----|-----|-----|-----|-----|-----|
| URBAN 15-30 MPH | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 |
| URBAN 35-45 MPH | 200 | 200 | 200 | 200 | 200 | 200 | 200 | 200 | 200 | 200 | 200 | 200 | 200 | 200 | 200 | 200 | 200 | 200 | 200 | 200 | 200 | 200 | 200 | 200 | 200 | 200 |
| URBAN 45-55 MPH | 300 | 300 | 300 | 300 | 300 | 300 | 300 | 300 | 300 | 300 | 300 | 300 | 300 | 300 | 300 | 300 | 300 | 300 | 300 | 300 | 300 | 300 | 300 | 300 | 300 | 300 |
| RURAL | 500 | 500 | 500 | 500 | 500 | 500 | 500 | 500 | 500 | 500 | 500 | 500 | 500 | 500 | 500 | 500 | 500 | 500 | 500 | 500 | 500 | 500 | 500 | 500 | 500 | 500 |

* PRECONSTRUCTION POSTED SPEED LIMIT

| ROAD TYPE | NON-FREEMAN | FREEMAN |
|-----------|-------------|---------|
| SD-2 | 36x18 | 48x24 |
| RD-1 | 36x36 | 48x48 |
| RD-2 | 36x36 | 48x48 |
| RD-3 | 36x36 | 48x48 |
| RD-4 | 36x36 | 48x48 |
| RD-5 | 36x36 | 48x48 |
| RD-6 | 36x36 | 48x48 |
| RD-7 | 36x36 | 48x48 |
| RD-8 | 36x36 | 48x48 |
| RD-9 | 36x36 | 48x48 |
| RD-10 | 36x36 | 48x48 |
| RD-11 | 36x36 | 48x48 |
| RD-12 | 36x36 | 48x48 |
| RD-13 | 36x36 | 48x48 |
| RD-14 | 36x36 | 48x48 |
| RD-15 | 36x36 | 48x48 |
| RD-16 | 36x36 | 48x48 |
| RD-17 | 36x36 | 48x48 |
| RD-18 | 36x36 | 48x48 |
| RD-19 | 36x36 | 48x48 |
| RD-20 | 36x36 | 48x48 |
| RD-21 | 36x36 | 48x48 |
| RD-22 | 36x36 | 48x48 |
| RD-23 | 36x36 | 48x48 |
| RD-24 | 36x36 | 48x48 |
| RD-25 | 36x36 | 48x48 |
| RD-26 | 36x36 | 48x48 |
| RD-27 | 36x36 | 48x48 |
| RD-28 | 36x36 | 48x48 |
| RD-29 | 36x36 | 48x48 |
| RD-30 | 36x36 | 48x48 |
| RD-31 | 36x36 | 48x48 |
| RD-32 | 36x36 | 48x48 |
| RD-33 | 36x36 | 48x48 |
| RD-34 | 36x36 | 48x48 |
| RD-35 | 36x36 | 48x48 |
| RD-36 | 36x36 | 48x48 |
| RD-37 | 36x36 | 48x48 |
| RD-38 | 36x36 | 48x48 |
| RD-39 | 36x36 | 48x48 |
| RD-40 | 36x36 | 48x48 |
| RD-41 | 36x36 | 48x48 |
| RD-42 | 36x36 | 48x48 |
| RD-43 | 36x36 | 48x48 |
| RD-44 | 36x36 | 48x48 |
| RD-45 | 36x36 | 48x48 |
| RD-46 | 36x36 | 48x48 |
| RD-47 | 36x36 | 48x48 |
| RD-48 | 36x36 | 48x48 |
| RD-49 | 36x36 | 48x48 |
| RD-50 | 36x36 | 48x48 |
| RD-51 | 36x36 | 48x48 |
| RD-52 | 36x36 | 48x48 |
| RD-53 | 36x36 | 48x48 |
| RD-54 | 36x36 | 48x48 |
| RD-55 | 36x36 | 48x48 |
| RD-56 | 36x36 | 48x48 |
| RD-57 | 36x36 | 48x48 |
| RD-58 | 36x36 | 48x48 |
| RD-59 | 36x36 | 48x48 |
| RD-60 | 36x36 | 48x48 |
| RD-61 | 36x36 | 48x48 |
| RD-62 | 36x36 | 48x48 |
| RD-63 | 36x36 | 48x48 |
| RD-64 | 36x36 | 48x48 |
| RD-65 | 36x36 | 48x48 |
| RD-66 | 36x36 | 48x48 |
| RD-67 | 36x36 | 48x48 |
| RD-68 | 36x36 | 48x48 |
| RD-69 | 36x36 | 48x48 |
| RD-70 | 36x36 | 48x48 |
| RD-71 | 36x36 | 48x48 |
| RD-72 | 36x36 | 48x48 |
| RD-73 | 36x36 | 48x48 |
| RD-74 | 36x36 | 48x48 |
| RD-75 | 36x36 | 48x48 |
| RD-76 | 36x36 | 48x48 |
| RD-77 | 36x36 | 48x48 |
| RD-78 | 36x36 | 48x48 |
| RD-79 | 36x36 | 48x48 |
| RD-80 | 36x36 | 48x48 |
| RD-81 | 36x36 | 48x48 |
| RD-82 | 36x36 | 48x48 |
| RD-83 | 36x36 | 48x48 |
| RD-84 | 36x36 | 48x48 |
| RD-85 | 36x36 | 48x48 |
| RD-86 | 36x36 | 48x48 |
| RD-87 | 36x36 | 48x48 |
| RD-88 | 36x36 | 48x48 |
| RD-89 | 36x36 | 48x48 |
| RD-90 | 36x36 | 48x48 |
| RD-91 | 36x36 | 48x48 |
| RD-92 | 36x36 | 48x48 |
| RD-93 | 36x36 | 48x48 |
| RD-94 | 36x36 | 48x48 |
| RD-95 | 36x36 | 48x48 |
| RD-96 | 36x36 | 48x48 |
| RD-97 | 36x36 | 48x48 |
| RD-98 | 36x36 | 48x48 |
| RD-99 | 36x36 | 48x48 |
| RD-100 | 36x36 | 48x48 |

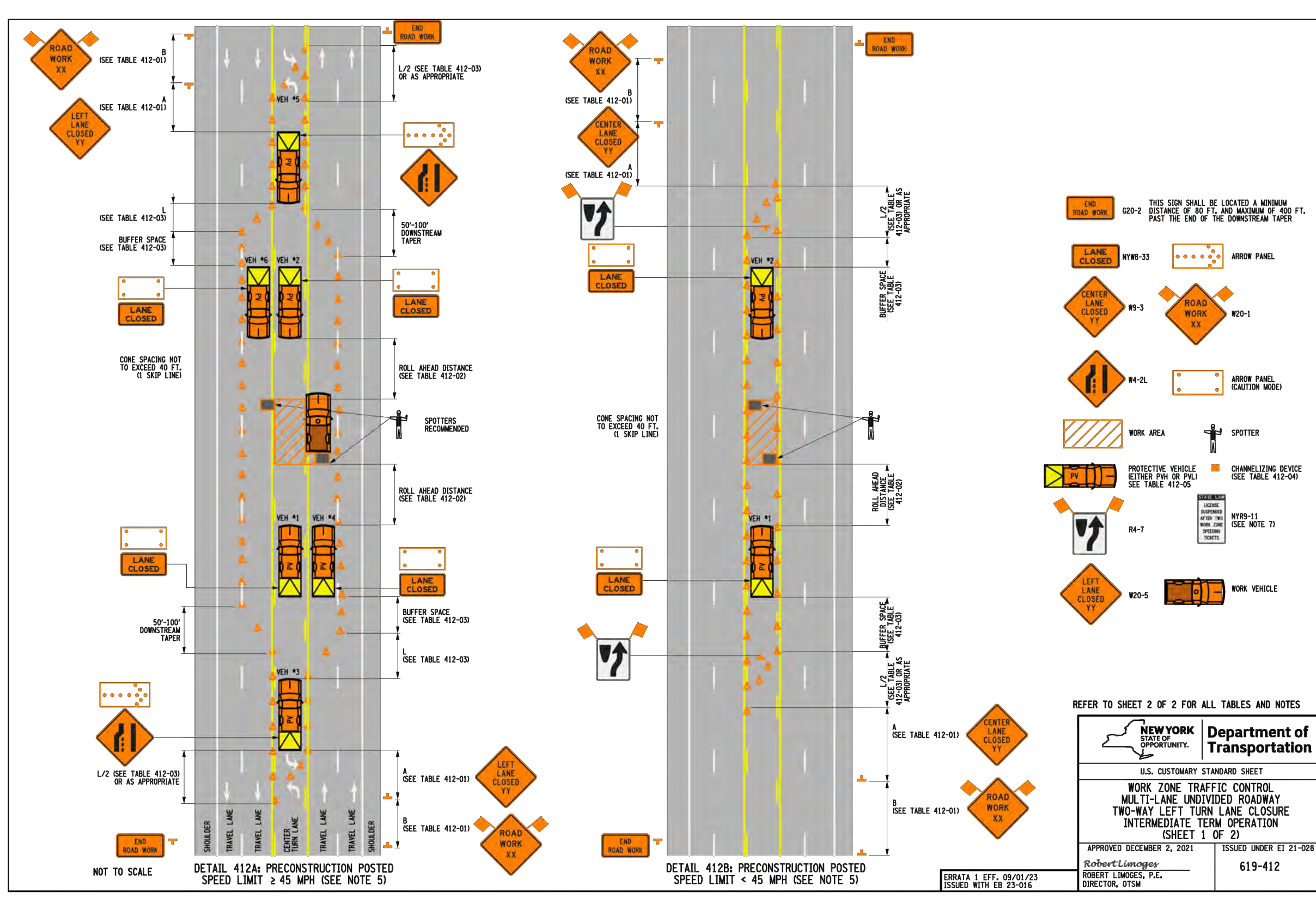
* FREEMAN SIZES MAY BE USED ON NON-FREEMAN, IF SPACE CONSTRAINTS DO NOT EXIST.

| WORK ZONE PROVISIONS | MISO COMPLIANT CHANNELIZING DEVICE | MISO COMPLIANT CHANNELIZING DEVICE | | | | | | | | | | | | | | | | | | | | | | | | |
|--|------------------------------------|------------------------------------|----------------|------------|------------------|-------------------------------------|-----------------|-----------------|-----------------------------|---------------------|---------------------|---|---|---|---|---|---|---|---|---|---|---|---|---|---|---|
| | | DRUM | STANDARD CONES | TALL CONES | EXTRA TALL CONES | TEMPORARY TRUCK MOUNTED ATTENUATORS | TUBULAR MARCHES | VERTICAL PANELS | UNDEVELOPED VERTICAL PANELS | TYPE III BARRICADES | TYPE III BARRICADES | | | | | | | | | | | | | | | |
| SHORT-TERM STATIONARY WORK ZONES INVOLVE WORK THAT OCCUPIES A LOCATION FOR MORE THAN 1 HOUR WITHIN A SINGLE DAYLIGHT PERIOD. | | | | | | | | | | | | | | | | | | | | | | | | | | |
| SHOULDER/WEARING/SLOTTING TAPERS | | 20 FT. | X | X | X | X | X | X | X | X | X | X | X | X | X | X | X | X | X | X | X | X | X | X | X | X |
| MARKING FOR TRANSVERSE BUMPS ¹ | | N/A | X | X | X | X | X | X | X | X | X | X | X | X | X | X | X | X | X | X | X | X | X | X | X | X |
| LONGITUDINAL LINE OR SHOULDER CLOSURE WHEN WORKERS PRESENT | | 80 FT. | X | X | X | X | X | X | X | X | X | X | X | X | X | X | X | X | X | X | X | X | X | X | X | X |
| TRANSVERSE DEVICE WITHIN CLOSED TRAFFIC LANE AND/OR SHOULDER | | 800 FT. | X | X | X | X | X | X | X | X | X | X | X | X | X | X | X | X | X | X | X | X | X | X | X | X |
| REMOVAL OF EXISTING GUIDE RAIL | | 80 FT. | X | X | X | X | X | X | X | X | X | X | X | X | X | X | X | X | X | X | X | X | X | X | X | X |
| REMOVAL OF EXISTING GUIDE RAIL | | 40 FT. | X | X | X | X | X | X | X | X | X | X | X | X | X | X | X | X | X | X | X | X | X | X | X | X |

NOTES: 1. - A TYPE I OBJECT MARKER MAY BE USED IN LIEU OF CHANNELIZING DEVICE.
2. - CHANNELIZING DEVICES SHALL BE EQUIPPED WITH A FLASHING WARNING LIGHT.

| PRECONSTRUCTION POSTED SPEED LIMIT (MPH) | LONGITUDINAL BUFFER SPACE DISTANCE (FT.) ¹ | STOPPING SIGHT DISTANCE (SD) (FT.) ² |
|--|---|---|
| 25 | 155/4 | 155/4 |
| 30 | 200/5 | 200/5 |
| 35 | 250/6 | 250/6 |
| 40 | 305/8 | 305/8 |
| 45 | 360/9 | 360/9 |
| 50 | 425/11 | 425/11 |
| 55 | 495/13 | 495/13 |

NEW YORK STATE Department of Transportation
U.S. CUSTOMARY STANDARD SHEET
WORK ZONE TRAFFIC CONTROL
TWO-LANE TWO-WAY ROADWAY
FLAGGING OPERATION AT INTERSECTION
SHORT TERM OPERATION
(SHEET 3 OF 3)
APPROVED JANUARY 10, 2025 ISSUED UNDER ET 25-001
Robert C. Longinoso, P.E. DIRECTOR, OTSM 619-323



NEW YORK STATE Department of Transportation
U.S. CUSTOMARY STANDARD SHEET
WORK ZONE TRAFFIC CONTROL
MULTI-LANE UNDIVIDED ROADWAY
TWO-WAY LEFT TURN LANE CLOSURE
INTERMEDIATE TERM OPERATION
(SHEET 1 OF 2)
APPROVED DECEMBER 2, 2021 ISSUED UNDER ET 21-028
Robert C. Longinoso, P.E. DIRECTOR, OTSM 619-412

| ROAD TYPE | A | B | C | D | E | F | G | H | I | J | K | L | M | N | O | P | Q | R | S | T | U | V | W | X | Y | Z |
|-----------------|-----|-----|-----|-----|-----|-----|-----|-----|-----|-----|-----|-----|-----|-----|-----|-----|-----|-----|-----|-----|-----|-----|-----|-----|-----|-----|
| URBAN 15-30 MPH | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 | 100 |
| URBAN 35-45 MPH | 200 | 200 | 200 | 200 | 200 | 200 | 200 | 200 | 200 | 200 | 200 | 200 | 200 | 200 | 200 | 200 | 200 | 200 | 200 | 200 | 200 | 200 | 200 | 200 | 200 | 200 |
| URBAN 45-55 MPH | 300 | 300 | 300 | 300 | 300 | 300 | 300 | 300 | 300 | 300 | 300 | 300 | 300 | 300 | 300 | 300 | 300 | 300 | 300 | 300 | 300 | 300 | 300 | 300 | 300 | 300 |
| RURAL | 500 | 500 | 500 | 500 | 500 | 500 | 500 | 500 | 500 | 500 | 500 | 500 | 500 | 500 | 500 | 500 | 500 | 500 | 500 | 500 | 500 | 500 | 500 | 500 | 500 | 500 |

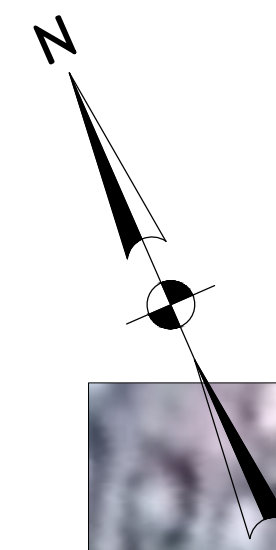
* PRECONSTRUCTION POSTED SPEED LIMIT

| PRECONSTRUCTION POSTED SPEED LIMIT (MPH) | STATIONARY OPERATION | | PROTECTIVE VEHICLES WEIGHING 5,000 TO 21,999 LBS. GVW | | PROTECTIVE VEHICLES WEIGHING 22,000 LBS. OR GREATER GVW | |
|--|---|---|---|---|---|---|
| | PROTECTIVE VEHICLES WEIGHING 5,000 TO 21,999 LBS. GVW | PROTECTIVE VEHICLES WEIGHING 22,000 LBS. OR GREATER GVW | PROTECTIVE VEHICLES WEIGHING 5,000 TO 21,999 LBS. GVW | PROTECTIVE VEHICLES WEIGHING 22,000 LBS. OR GREATER GVW | PROTECTIVE VEHICLES WEIGHING 5,000 TO 21,999 LBS. GVW | PROTECTIVE VEHICLES WEIGHING 22,000 LBS. OR GREATER GVW |
| 45 - 55 | 160/4 | 120/3 | 160/4 | 120/3 | 160/4 | 120/3 |
| ≥ 40 | 120/3 | 80/2 | 120/3 | 80/2 | 120/3 | 80/2 |

| PRECONSTRUCTION POSTED SPEED LIMIT (MPH) | LONGITUDINAL BUFFER SPACE DISTANCE (FT.) ¹ | TAPER LENGTH (FT.) ² OF CHANNELIZING DEVICES | | | | | | | | | | | | | | | | | | | | | | | |
|--|---|---|-----------|-----------|--------|---------|--------------------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|
| | | FOR LANE WIDTH IN FT. LATERAL SHIFT OF TRAFFIC FLOW PATTERN | | | | | FOR SHOULDER WIDTH | | | | | | | | | | | | | | | | | | |
| 25 | 155/4 | 120/3/4 | 110/3 | 120/3/4 | 40/1/2 | 40/1/2 | 40/1/2 | 40/1/2 | 40/1/2 | 40/1/2 | 40/1/2 | 40/1/2 | 40/1/2 | 40/1/2 | 40/1/2 | 40/1/2 | 40/1/2 | 40/1/2 | 40/1/2 | 40/1/2 | 40/1/2 | 40/1/2 | 40/1/2 | 40/1/2 | 40/1/2 |
| 30 | 200/5 | 160/4/5 | 160/4/5 | 200/5/6 | 40/1/2 | 40/1/2 | 40/1/2 | 40/1/2 | 40/1/2 | 40/1/2 | 40/1/2 | 40/1/2 | 40/1/2 | 40/1/2 | 40/1/2 | 40/1/2 | 40/1/2 | 40/1/2 | 40/1/2 | 40/1/2 | 40/1/2 | 40/1/2 | 40/1/2 | 40/1/2 | 40/1/2 |
| 35 | 250/6 | 200/5/6 | 210/5/7 | 210/5/7 | 40/1/2 | 40/1/2 | 40/1/2 | 40/1/2 | 40/1/2 | 40/1/2 | 40/1/2 | 40/1/2 | 40/1/2 | 40/1/2 | 40/1/2 | 40/1/2 | 40/1/2 | 40/1/2 | 40/1/2 | 40/1/2 | 40/1/2 | 40/1/2 | 40/1/2 | 40/1/2 | 40/1/2 |
| 40 | 305/8 | 260/7/8 | 300/8/9 | 300/8/9 | 40/1/2 | 40/1/2 | 40/1/2 | 40/1/2 | 40/1/2 | 40/1/2 | 40/1/2 | 40/1/2 | 40/1/2 | 40/1/2 | 40/1/2 | 40/1/2 | 40/1/2 | 40/1/2 | 40/1/2 | 40/1/2 | 40/1/2 | 40/1/2 | 40/1/2 | 40/1/2 | 40/1/2 |
| 45 | 360/9 | 440/11/12 | 520/13/14 | 560/14/15 | 80/2/3 | 80/2/3 | 120/3/4 | 120/3/4 | 120/3/4 | 120/3/4 | 120/3/4 | 120/3/4 | 120/3/4 | 120/3/4 | 120/3/4 | 120/3/4 | 120/3/4 | 120/3/4 | 120/3/4 | 120/3/4 | 120/3/4 | 120/3/4 | 120/3/4 | 120/3/4 | 120/3/4 |
| 50 | 425/11 | 520/13/14 | 560/14/15 | 600/15/16 | 80/2/3 | 120/3/4 | 160/4/5 | 160/4/5 | 160/4/5 | 160/4/5 | 160/4/5 | 160/4/5 | 160/4/5 | 160/4/5 | 160/4/5 | 160/4/5 | 160/4/5 | 160/4/5 | 160/4/5 | 160/4/5 | 160/4/5 | 160/4/5 | 160/4/5 | 160/4/5 | 160/4/5 |
| 55 | 495/13 | 560/14/15 | 600/15/16 | 640/17/18 | 80/2/3 | 120/3/4 | 160/4/5 | 160/4/5 | 160/4/5 | 160/4/5 | 160/4/5 | 160/4/5 | 160/4/5 | 160/4/5 | 160/4/5 | 160/4/5 | 160/4/5 | 160/4/5 | 160/4/5 | 160/4/5 | 160/4/5 | 160/4/5 | 160/4/5 | 160/4/5 | 160/4/5 |

| WORK ZONE PROVISIONS | MISO COMPLIANT CHANNELIZING DEVICE | MISO COMPLIANT CHANNELIZING DEVICE | | | | | | | | | |
|--|------------------------------------|------------------------------------|----------------|------------|------------------|-------------------------------------|-----------------|-----------------|-----------------------------|---------------------|---------------------|
| | | DRUM | STANDARD CONES | TALL CONES | EXTRA TALL CONES | TEMPORARY TRUCK MOUNTED ATTENUATORS | TUBULAR MARCHES | VERTICAL PANELS | UNDEVELOPED VERTICAL PANELS | TYPE III BARRICADES | TYPE III BARRICADES |
| INTERMEDIATE-TERM STATIONARY WORK ZONES INVOLVE WORK THAT OCCUPIES A LOCATION FOR MORE THAN 1 HOUR WITHIN A SINGLE DAYLIGHT PERIOD UP TO 3 CONSECUTIVE DAYS OF NIGHTTIME WORK THAT OCCUPIES A LOCATION FOR MORE THAN 1 HOUR. | | | | | | | | | | | |

NOTES:
 1. REFER TO WORK ZONE TRAFFIC CONTROL DETAIL SHEETS FOR DETAILS.

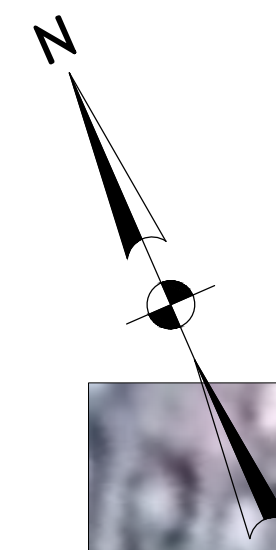


WORK ZONE AT MILLERSPORT HIGHWAY
 NORTH FOREST WESTBOUND PAVING
 NOT TO SCALE

V:\Public Works\Highways - Main\Road Files\Overlays\2026 Overlay Program\2026WZTC.dwg

| | | |
|-------------------------|---|--------------------|
| DRAWN BY: DSJ | ERIE COUNTY DEPT. OF PUBLIC WORKS | SHEET NO. 39 OF 41 |
| CHECKED BY: GMW | DIVISION OF HIGHWAYS | DATE: MAY 26 |
| APPROVED BY: DMS | 95 FRANKLIN ST. BUFFALO, N.Y. | SCALE: NONE |
| DATE COMPLETED: 5/13/26 | CAPITAL OVERLAY B WORK ZONE TRAFFIC CONTROL DETAILS (8 OF 9) | WZD-8 |

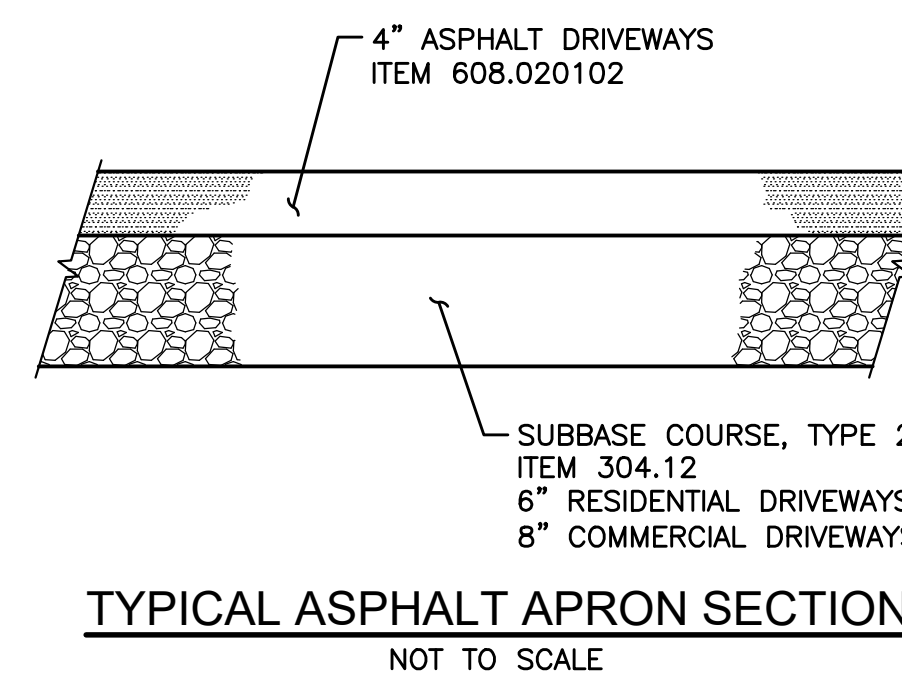
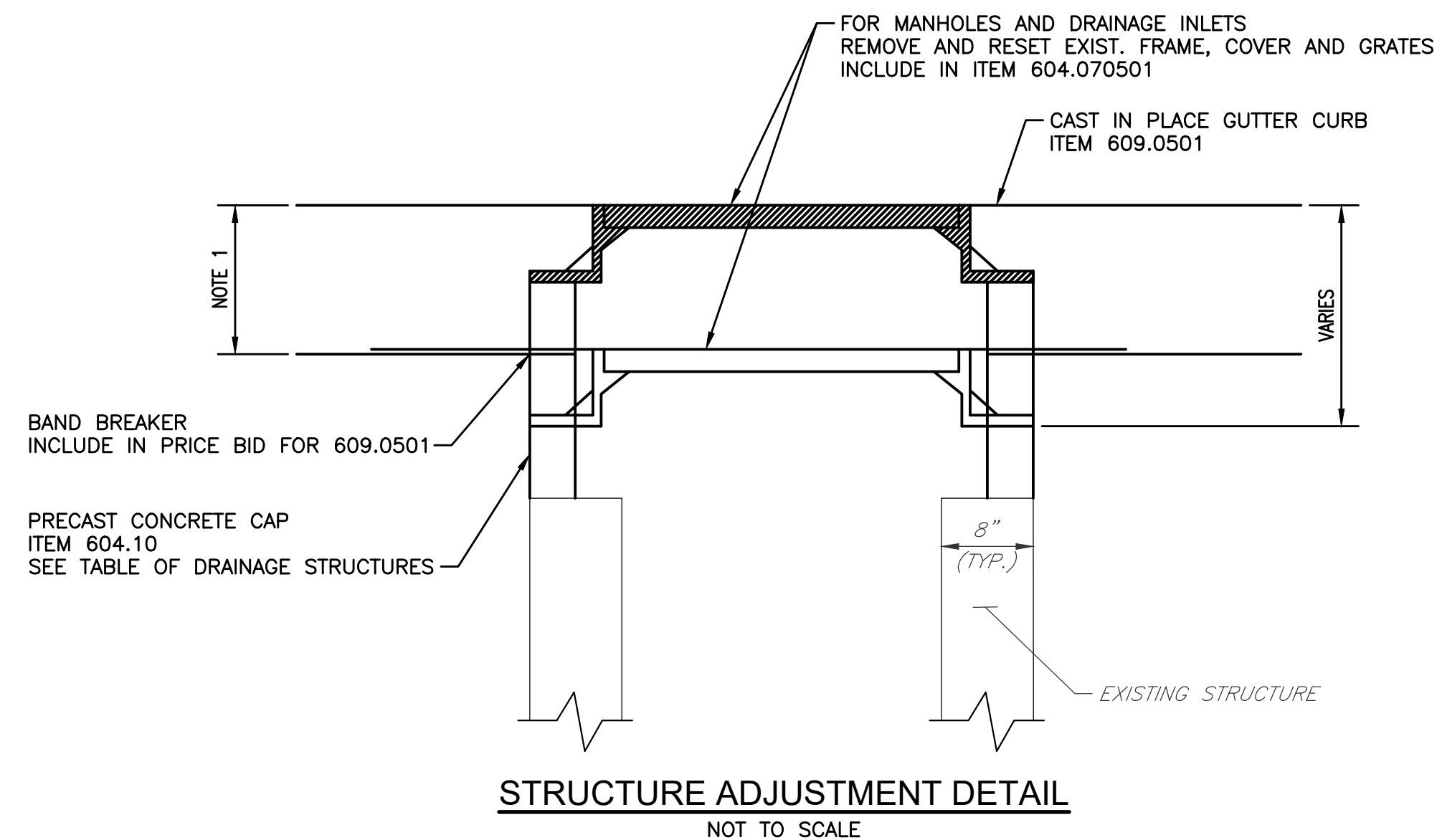
NOTES:
 1. REFER TO WORK ZONE TRAFFIC CONTROL DETAIL SHEETS FOR DETAILS.



WORK ZONE AT MILLERSPORT HIGHWAY
 NORTH FOREST EASTBOUND PAVING
 NOT TO SCALE

V:\Public Works\Highways - Main\Road Files\Overlays\2026 Overlay Program\2026WZTC.dwg

| | | |
|-------------------------|---|--------------------|
| DRAWN BY: DSJ | ERIE COUNTY DEPT. OF PUBLIC WORKS | SHEET NO. 40 OF 41 |
| CHECKED BY: GMW | DIVISION OF HIGHWAYS | DATE: MAY 26 |
| APPROVED BY: DMS | 95 FRANKLIN ST. BUFFALO, N.Y. | SCALE: NONE |
| DATE COMPLETED: 5/13/26 | CAPITAL OVERLAY B WORK ZONE TRAFFIC CONTROL DETAILS (9 OF 9) | WZD-9 |



NOTES:

1. THE COST FOR ANY EXCAVATION NECESSARY TO ALTER DRAINAGE STRUCTURES OR MANHOLES, BACKFILL INCLUDING TEMPORARY RESTORATION, IS INCLUDED IN THE PRICE BID FOR ALTERATION ITEMS 604.07050Y ('Y' IS SERIALIZED FOR CONTRACT ITEMS).
2. ALL STRUCTURES TO BE ALTERED AS PART OF THIS CONTRACT SHALL BE CLEANED AS NECESSARY. THE COST FOR CLEANING IS INCLUDED IN THE PRICE BID FOR ITEM(S) - 604.07050Y.
3. A MINIMUM OF 6" PRECAST CONCRETE PAVERS AND MORTAR FOR CONCRETE, OR MAXIMUM OF 2" OF MORTAR FOR CONCRETE ALONE SHALL BE USED FOR FRAME AND GRATE ADJUSTMENT. FOR ADJUSTMENTS GREATER THAN 6", AND NOT TO EXCEED 12", CAST-IN-PLACE CONCRETE OR A PRECAST CONCRETE ADJUSTMENT ELEMENT SHALL BE USED. A MAXIMUM OF 2" OF MORTAR FOR CONCRETE SHALL BE ALLOWED ON BOTH THE TOP AND BOTTOM OF THE PRECAST DEVICES.

V:\Public Works\Highways - Main\Road Files\Overlays\2026 Overlay Program

CONCRETE CURB AND GUTTER DETAIL
 NOT TO SCALE

| | | | |
|----------------|---------|---|--------------------|
| DRAWN BY | DSU | ERIE COUNTY DEPT. OF PUBLIC WORKS | SHEET NO. 41 OF 41 |
| CHECKED BY | GMW | DIVISION OF HIGHWAYS | DATE MAY 26 |
| APPROVED BY | DMS | 95 FRANKLIN ST BUFFALO, N.Y. | SCALE NONE |
| DATE COMPLETED | 5/13/26 | 2026 CAPITAL OVERLAY B MISCELLANEOUS DETAILS | MDS-1 |